

CARROLL COUNTY

09-SR436-0068

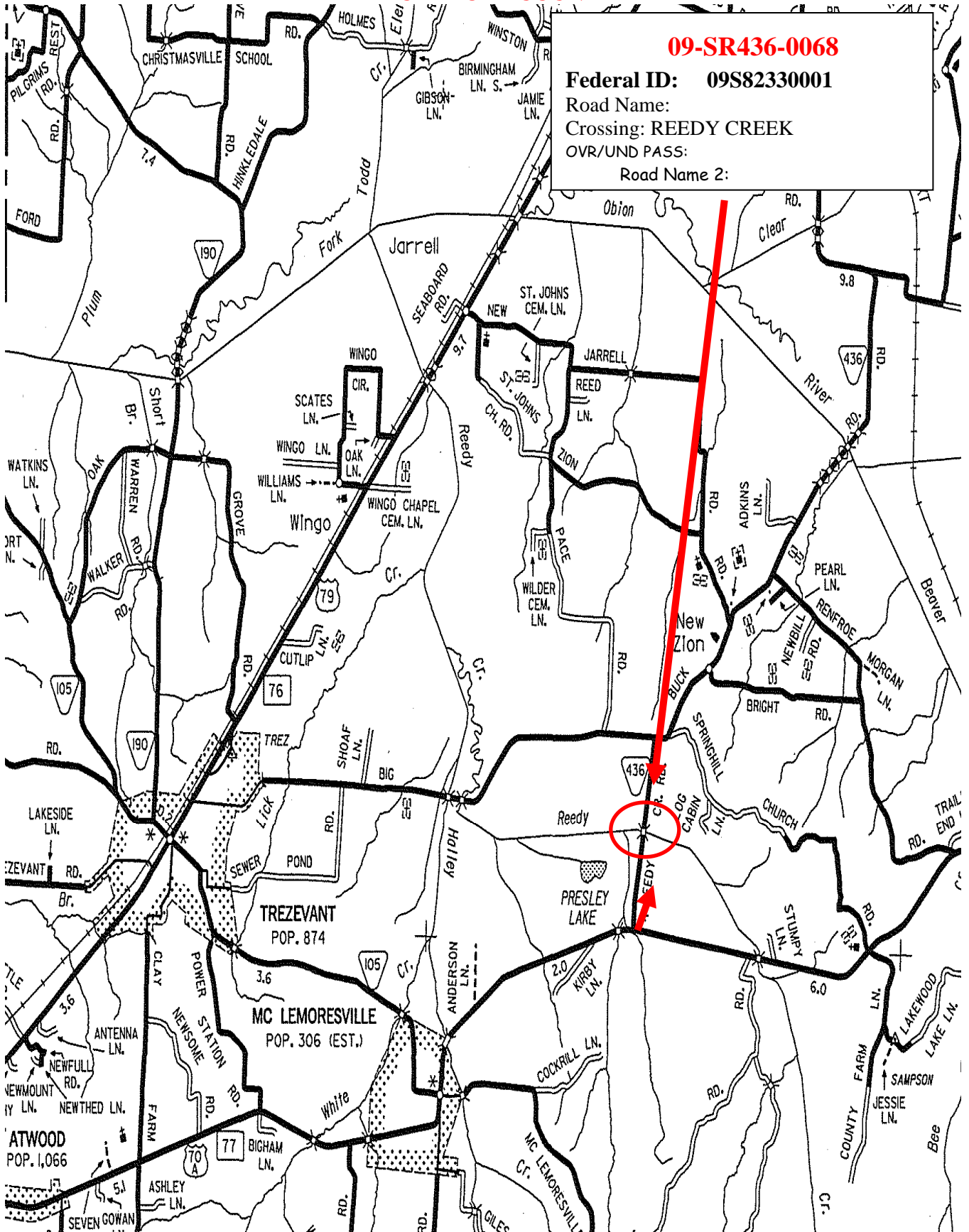
Federal ID: 09S82330001

Road Name:

Crossing: REEDY CREEK

OVR/UND PASS:

Road Name 2:





STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

Bridge Condition Coding Form

Revised 08/19/2019

Bridge Number: 09S823300011
(Includes Item 5A)

Feature Intersected: REEDY CREEK

Evaluation Status: OTHER ITEM(S) HAVE BEEN CHANGED

County: 9

Route: SR436

Special Case: 0

County Sequence: 1

Log Mile: 0.68

CODE ONLY THOSE VALUES WHICH HAVE CHANGED

ITEM #	DESCRIPTION	VALUE	CONDITION CODING GUIDELINES
90	LAST INSPECTION DATE	<u>08/15/2019</u>	(Values for Coding Items 58, 59, 60 and 62)
	EARLIEST DATE OF NEXT REGULAR INSPECTION	<u>06/15/2021</u>	
		<u>/ /</u>	
10	MINIMUM V.C. OVER DECK (ROADWAY + SHOULDERS)	99 FT. 99 IN.	N NOT APPLICABLE
520	MINIMUM V.C. OVER DECK (EXCLUDES SHOULDERS)	99 FT. 99 IN.	9 EXCELLENT CONDITION
36	TRAFFIC SAFETY FEATURES		8 VERY GOOD CONDITION - NO PROBLEMS NOTED.
	Br. Rail Trans. Appr. Rail Terminal SPEED LIMIT	0 0 0 0 45	7 GOOD CONDITION - SOME MINOR PROBLEMS.
41	STRC OPEN/CLOSED/POSTED	P	6 SATISFACTORY CONDITION - MINOR DETERIORATION OF STRUCTURAL ELEMENTS.
	A K P		5 FAIR CONDITION - ALL PRIMARY STRUCTURAL ELEMENTS ARE SOUND BUT MAY HAVE MINOR SECTION LOSS, CRACKING, SPALLING OR SCOUR.
58	DECK	5	4 POOR CONDITION - ADVANCED SECTION LOSS, DETERIORATION, SPALLING OR SCOUR.
59	SUPERSTRUCTURE	5	3 SERIOUS CONDITION - LOSS OF SECTION, DETERIORATION, SPALLING OR SCOUR HAVE SERIOUSLY AFFECTED PRIMARY STRUCTURAL COMPONENTS. LOCAL FAILURES ARE POSSIBLE. FATIGUE CRACKS IN STEEL OR SHEAR CRACKS IN CONCRETE MAY BE PRESENT.
60	SUBSTRUCTURE	5	2 CRITICAL CONDITION - ADVANCED DETERIORATION OF PRIMARY STRUCTURAL ELEMENTS. FATIGUE CRACKS IN STEEL OR SHEAR CRACKS IN CONCRETE MAY BE PRESENT OR SCOUR MAY HAVE REMOVED SUBSTRUCTURE SUPPORT. UNLESS CLOSELY MONITORED IT MAY BE NECESSARY TO CLOSE THE BRIDGE UNTIL CORRECTIVE ACTION IS TAKEN.
61	CHANL/CHANL PROTECTION	6	1 "IMMINENT" FAILURE CONDITION - MAJOR DETERIORATION OR SECTION LOSS PRESENT IN CRITICAL STRUCTURAL COMPONENTS OR OBVIOUS VERTICAL OR HORIZONTAL MOVEMENT AFFECTING STRUCTURAL STABILITY. BRIDGE IS CLOSED TO TRAFFIC BUT CORRECTIVE ACTION MAY PUT IT BACK IN LIGHT SERVICE.
62	CULVERT AND RETAIN WALL	N	0 FAILED CONDITION - OUT OF SERVICE AND BEYOND CORRECTIVE ACTION
71	WATERWAY ADEQUACY	6	
72	APPROACH RDWY ALIGNMENT	8	
521	OVERALL CONDITION	<u>FAIR</u>	
16	LATITUDE	17 LONGITUDE	
	N 36° .8600'	W 88° 32.4583'	

TEAM LEADER SIGNATURE

REVIEW DATE

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BRIDGE MAINTENANCE RECOMMENDATIONS

COUNTY: CARROLL

LOCATION: 9-SR436-00.68-

CO. SEQ.: 1 SPEC. CASE: 0

Tennessee Department
of Transportation

CROSSING: REEDY CREEK

FED. BRIDGE NO.: 09S82330001

MAINT. DIST.: 9

REPAIR LIST NO.: N

DATE ADDED: 03/12/2012

REVISED: 08/15/2019

FACILITY CARRIED:	FAS 436	NUMBER OF MAIN SPANS:	1
HIGHWAY SYSTEM:	05-STP RURAL, STATE	NUMBER OF APPROACH SPANS:	3
BRIDGE WIDTH (CURB TO CURB):	20 FT 4 IN	BRIDGE LENGTH (FT):	90
BRIDGE WIDTH (OUT TO OUT):	21 FT 7 IN	MAXIMUM SPAN LENGTH (FT):	33
APPROACH ROADWAY (W/SHOULDERS):	27 FT 10 IN	SKEW ANGLE (DEGREES):	90
MAINTAINED BY:	STATE HIGHWAY AGENCY		
MAIN SPAN MATERIAL:	PRESTRESSED CONCRETE		
MAIN SPAN DESIGN TYPE:	BOX BEAM OR GIRDERS - MULTIPLE		
APPROACH SPAN MATERIAL:	CONCRETE		
APPROACH SPAN DESIGN TYPE:	SLAB		
INSPECTION DATE:	08/15/2019	GENERAL CONDITION:	FAIR
EVALUATION DATE:	10/09/2017	STRUCTURALLY DEFICIENT:	NO
PPRM PIN NUMBER:	124139.00		
H TRUCK RATING @ INV.:	15 TONS	SUFFICIENCY RATING:	60.2

SUGGESTED ROUTINE MAINTENANCE AND COMMENTS

REPAIR CAPBEAM AT BENT NO.1B

CUT AND REMOVE VEGETATION FROM CHANNEL

APPROACH GUARDRAILS ARE NON-EXISTENT

BRIDGERAILS ARE SUBSTANDARD

GENERAL COMMENTS:

BRIDGE IS ON THE IMPROVE ACT. EARLIEST LETTING DATE FOR REPLACEMENT IS CY 2021.

Bridge Loc. No: 09-SR436-00.68 Date: 8-15-19



BRIDGE NUMBER



RIGHT SIDE

Bridge Loc. No: 09-SR436-00.68 Date: 8-15-19



APPROACH # 1



APPROACH # 1 WEIGHT LIMIT SIGN

Bridge Loc. No: 09-SR436-00.68 Date: 8-15-19



LOOKING AHEAD ON ROUTE

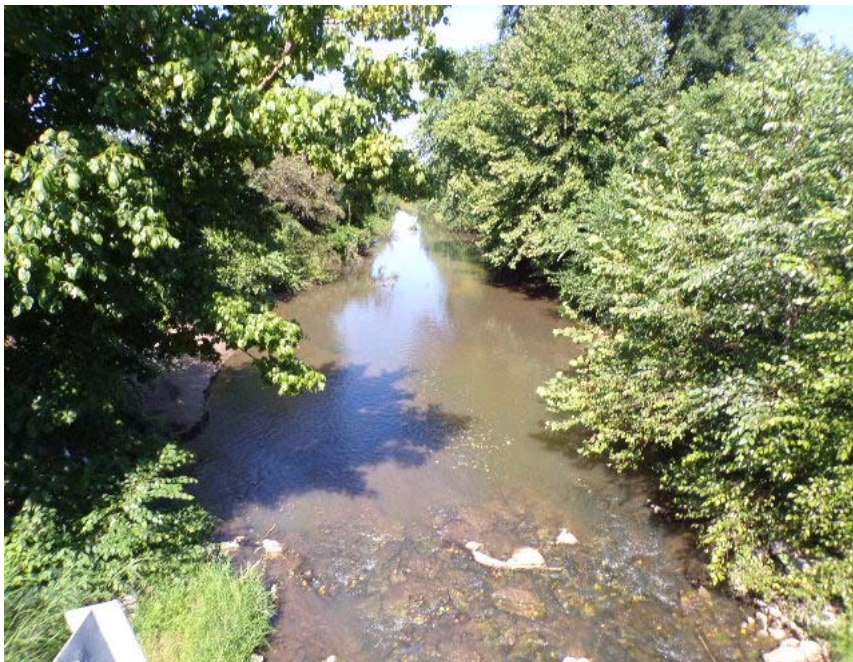


SPAN # 1 SETTLING

Bridge Loc. No: 09-SR436-00.68 Date: 8-15-19



SPAN # 1



LEFT SIDE

Bridge Loc. No: 09-SR436-00.68 Date: 8-15-19



SPAN # 4 VIEW ACROSS TOP OF DECK



APPROACH # 2

Bridge Loc. No: 09-SR436-00.68 Date: 8-15-19



APPROACH # 2 WEIGHT LIMIT SIGN/LOOKING BACK ON ROUTE



BENT # 1

Bridge Loc. No: 09-SR436-00.68 Date: 8-15-19



ABUTMENT # 1



SPAN # 1 BOTTOM DECK/ SPANS # 3 & 4 TYPICAL

Bridge Loc. No: 09-SR436-00.68 Date: 8-15-19



SPAN # 2 BOTTOM DECK



BENT # 1 "B" CAP DECAYED

Bridge Loc. No: 09-SR436-00.68 Date: 8-15-19



BENT # 2 "A"



RIGHT SIDE ELEVATION

Bridge Loc. No: 09-SR436-00.68 Date: 8-15-19



LEFT SIDE ELEVATION



BENT # 2 DRIFT

Bridge Loc. No: 09-SR436-00.68 Date: 8-15-19



BENT # 2 "B"



ABUTMENT # 2

Bridge Loc. No: 09-SR436-00.68 Date: 8-15-19



BENT # 3 PILE "C" SPLIT



BENT # 3

AUG 15 2019

BRIDGE INSPECTION REPORTForm BIR 3.0
(Rev. 9-22-98)
DT-0069Field Report No.: _____ Date: 8/15/19
Previous Report No.: 24 Date: 10/2/17
Co. Seq 01 Plans: YES (X) NO ()Bridge No. 09S82330001 Bridge Location No. 09 - SR436 - 0068
Eleven Digit No. Co. Route Log Mile OVER/UNDER PASSRoad Name _____ Feature Intersected REEDY CREEK CITY _____
Year Constructed 1960 County Carroll Maint. Dist: 47 Maint.Resp: 02
Year Widened _____ Year Rehabilitated _____

Structure Name (If Named) _____

FEATURESWearing Surface Concrete () Timber () Asphalt (X) Depth 1 1/2" (in.)
Flared Width Yes () No (X) Median Width Open () None (X) Closed ()
Navigational Control Yes () No (X) Bridge Skew 90 ° LT () RT ()
Structure Type (Main Span) CONCRETE BOX BEAM & P. C. C. S.
Structure Type (Appr.Spans) _____
No. Main Spans 4 No. Approach Spans _____
Maximum Span Length 33.0 (**. * ft.)
Total Length 91.0 (**. * ft.)**INSPECTORS**

- 1.
- L Jones TL
-
- 2.
- Ferrell
-
- 3.
- Thomas
-
- 4.
- Hayes
-
- 5.
- Prince
-
6. _____
-
7. _____
-
8. _____

WIDTHS (*. * ft.)Deck Out-to-Out 22.0
Roadway Curb/Curb 21.0
Roadway Rail/Rail _____
Sidewalk Rt. _____ Lt. _____
*Approach Roadway 18.0
*(Does Not Include Shoulders)
Approach Shoulder Rt. 3.0
Lt. 3.0**CLEARANCES**Min. Vertical Clearance over Deck _____ (ft.-in.)
Min. Vertical Under Clearance _____ (ft.-in.)
Min. Lateral Under Clearance Rt. _____ (*. * ft.)
Min. Lateral Under Clearance Lt. _____ (*. * ft.)**FRACTURE CRITICAL:**

(If Yes, Include BIR 3.9)

NBIS Bridge Length (<25 ft.) _____ (ft.-in.)

UNDERWATER INSPECTION

To Be Performed By: _____ Date _____

DOT FIELD TEAM () CONTRACT DIVERS () NONE REQUIRED (X)

Change in Structural Condition: Yes () No (X) Major Repairs Made: Yes (X) No ()

COMMENTS:Substructure repairsLATITUDE: N36 ° 0.8600LONGITUDE: W88 ° 32.4583

G.P.S. Location

BRIDGE RATING: () (X) () ()

GOOD FAIR POOR CRITICAL

Supervising Bridge Inspector: _____

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PERFORMANCE EVALUATION

Time of Day Inspected 9:55 AM Weather Conditions Clear 80°
Vehicles Observed Normal Traffic

LIVE LOAD BEHAVIOR

	YES	NO	Comments
Substructure			
Horiz./ Vert. Defl.	()	(X)	
Vibration	()	(X)	
Superstructure			
Horiz./ Vert. Defl.	()	(X)	
Vibration	()	(X)	

APPROACH

	Rating	Comments
Alignment	Ⓒ F P C	
Slab	G F P C	N/A
Joints	G F P C	
Pavement	G Ⓕ P C	Light settling & Fine Cracks
Embankment	Ⓒ F P C	
Drains	G F P C	N/A

TRAFFIC SAFETY FEATURES

	Rating	STANDARD/ SUB-STANDARD	Comments
Bridgerailing	Ⓒ F P C	() (X)	
Transitions	G F P C	() ()	N/A
Guardrail	G F P C	() ()	
Guardrail Terminal	G F P C	() ()	

SIGNING

	YES	NO	NEEDED	Weight Limit Posted
Paddleboards	(X)	()	()	YES (X) NO ()
Vertical Clearance (<14'-6")	()	(X)	()	Gross..... <u>40</u> Tons
NARROW ()	()	(X)	()	2 Axle..... <u>40</u> Tons
ONE LANE BRIDGE ()	()	(X)	()	3 or more Axles.. _____ Tons

Other Signs or Plaques: _____

Comments Regarding any Problems with Signing: _____

DECK

	Rating				Comments
Wearing Surface	G	<u>F</u>	P	C	
Deck - Structural Condition	G	<u>F</u>	P	C	
Curbs	G	<u>F</u>	P	C	
Median	G	F	P	C	
Sidewalks	G	F	P	C	
Parapet	G	F	P	C	
Railing	<u>G</u>	F	P	C	
Paint	G	F	P	C	
Drains	<u>G</u>	F	P	C	
Lighting Standards	G	F	P	C	
Utilities	G	F	P	C	
Joint Leakage	G	F	P	C	
Expansion Joints	G	F	P	C	

SUPERSTRUCTURE

Bearing Devices	G	F	P	C	
Beams <i>LB B</i>	G	<u>F</u>	P	C	
Girders	G	F	P	C	
P C C S	G	<u>F</u>	P	C	
BOLTS (PCCS)	G	<u>F</u>	P	C	
Floor Beams	G	F	P	C	
Stringers	G	F	P	C	
Diaphragms	G	F	P	C	
Bracing	G	F	P	C	
Trusses - General	G	F	P	C	
Portals	G	F	P	C	
Bracing	G	F	P	C	
Paint	G	F	P	C	
Alignment of Members	<u>G</u>	F	P	C	

TEXTURE COAT

Condition Rating	G	F	P	C	Fading	G	F	P	C
Overall Appearance	<u>G</u>	<u>F</u>	<u>P</u>	<u>C</u>	Needs Spot Painting	<u>YES</u>	()	NO	()
Staining Rating	G	F	P	C	Needs Repainting	<u>YES</u>	()	NO	()
Comments					Scaling Rating	<u>G</u>	<u>F</u>	<u>P</u>	<u>C</u>
RECOMMENDATIONS:					CLEAN SEAL JOINTS	()			
					CLEAN DRAINS	()			

SUBSTRUCTURE

PILES TO BE
REPLACED

ABUTMENTS

	Rating	Comments	PILE(S)	ABUTMENT
Caps	G <u>F</u> P C			
Breastwall	G <u>F</u> P C			
Wings	G <u>F</u> P C			
Backwall	G F P C			
Plumb	G F P C			
Footings Veg.	G F <u>P</u> C	Heavy in General		(204)
Piles	G <u>F</u> P C			
Embankment	<u>G</u> F P C			
Bearing Ret wall	G <u>F</u> P C			
Slope Paving	G F P C			
Rip Rap	G <u>F</u> P C			
Earthquake Devices	G F P C			

PIERS

			PILE(S)	PIER
Caps	G F P C			
Columns	G F P C			
Plumb	G F P C			
Footings	G F P C			
Piles	G F P C			
Bearing	G F P C			
Web	G F P C			
Earthquake Devices	G F P C			

BENTS

			PILE(S)	BENT
Caps	G F <u>P</u> C	Bent #1 'B' Decayed		(135)
Columns Rip Rap	<u>G</u> F P C			
Plumb	<u>G</u> F P C			
Footings conc casing	<u>G</u> F P C			
Piles	G <u>F</u> P C			
Bearing	G F P C			
Bracing	G <u>F</u> P C			
Earthquake Devices	G F P C			

Piles Need Replacement: NO (X) YES ()

CUT VEGETATION NO () YES (X) (204)

CLEAR DRIFT NO (X) YES ()

RECOMMENDATIONS:

STREAM CHANNEL DATA AND CONDITIONS

Stream Crossing: REEDY CREEK

- I. 1. Type of bed material? Sand / silt
2. Has channel shifted? YES () NO (X) NOT APPARENT ()
3. Condition of rip-rap? G B P C Est. % failed 4/0 % N/A ()
4. Overall condition of channel? G F P C
5. Item 61 - Code values 0 thru 9 according to the recording
and coding guide currently in effect: 6
6. Underwater diver inspection recommended? YES () NO (X)
If yes, why? _____

- II. Channel and bank stability conditions: (check if applicable)
1. Steep bank conditions: - Failures upstream () Failures downstream ()
2. Moderate bank erosion (X)
3. Bank vegetation: a. low growth (X) b. large timber (X) c. clear banks ()
d. dead trees upstream (X) e. dead trees downstream (X)
4. Sediment or gravel accumulation: YES () NO (X) UNKNOWN ()
5. Channel altered or straightened: YES () NO (X) UNKNOWN ()
6. Stable conditions: a. live growth (X) b. bedrock ()
c. boulders () d. flat slopes ($\leq 2:1$) ()

- III. Waterway adequacy and debris characteristics: (check if applicable)
1. Bridge deck elevations:
a. level with approach roadway. (X)
b. higher than approach roadway. ()
c. roadway approach ≥ 2 ft. above natural ground line. (X)
2. Abutment encroaches into channel. ()
3. Large scour (blowhole) under bridge. ()
4. Indications that flood waters overtop bridge:
NO (X) YES () OCCASIONALLY () FREQUENTLY () UNKNOWN ()
5. Debris characteristics:
a. debris/drift present YES (X) NO ()
b. debris/drift likely to accumulate YES (X) NO ()
c. dead trees upstream (X) dead trees downstream (X)

IV. Comments: _____

SPECIAL INSPECTION DATA - FOR REASONS OTHER THAN FC OR SCOUR

- I. Does this bridge need a special inspection? YES () NO (X)
II. Reason for special inspection: _____

Inspection Team's Summary
Bridge Location No. 09 - SR436 - 00.68
Inspection Date 8-15-19
Bridge Rating FAIR

THIS IS A 4 SPAN CONCRETE BOX BEAM & P.C.C.S. BRIDGE IN FAIR
CONDITION
SUBSTRUCTURE IS TIMBER
TRAFFIC SAFETY FEATURES CONSIST OF METAL BRIGE RAILS, 4 PADDLE
BOARDS & 40 TON WEIGHT LIMIT POSTED @ APPROACH # 1 & 2
APPROACH A/C HAS FINE CRACKS & LIGHT SETTLING
A/C WEARING SURFACE HAS SPALLING, FINE CRACKS & IS SETTLED
BETWEEN SLABS
P.C.C.S. HAVE FINE CRACKS WITH EFFLORESCENCE STAINS & SPALLING
TO STEEL AREAS
CONCRETE BOX BEAMS HAVE SURFACE STEEL
BNET # 1 "B" CAP IS DECAYED
SUBSTRUCTURE HAS LIGHT TO MEDIUM WEATHERING
BENT # 3 PILE "C" HAS A SPLIT AREA

VEGETATION IS HEAVY IN GENERAL

NO PROBLEMS WITH SCOUR

LEONARD JONES

INSPECTOR

CROSS SECTION: YES (X) NO ()

BRM: YES (X) NO ()

LS
w/c
8/15/19

AUG 15 2019

GROUND ELEVATIONS

FEDERAL NUMBER ----- 09S82330001
BRIDGE NO. ----- 9-436-0.68
CROSSING ----- REEDY CREEK
NUMBER OF PIERS ----- 3
LOCATION OF PIERS ----- 19, 52, 71
BENCH MARK ELEV. ----- 105.93
BENCH MARK LOC. ---- TOP CAP RT. SIDE A-1
WATER ELEVATION -----

DATE : 9/13/2000
N/C 6/19/08

INSPECTORS
~~SCOTT'S CREW~~

L Jones crew

DISTANCE OF 0.00 = TOP OF BANK APPROACH 1 SIDE

DISTANCE AND ELEVATIONS ARE IN STANDARD MEASUREMENT

UPSTREAM GROUND ELEVATION @ EDGE OF BRIDGE

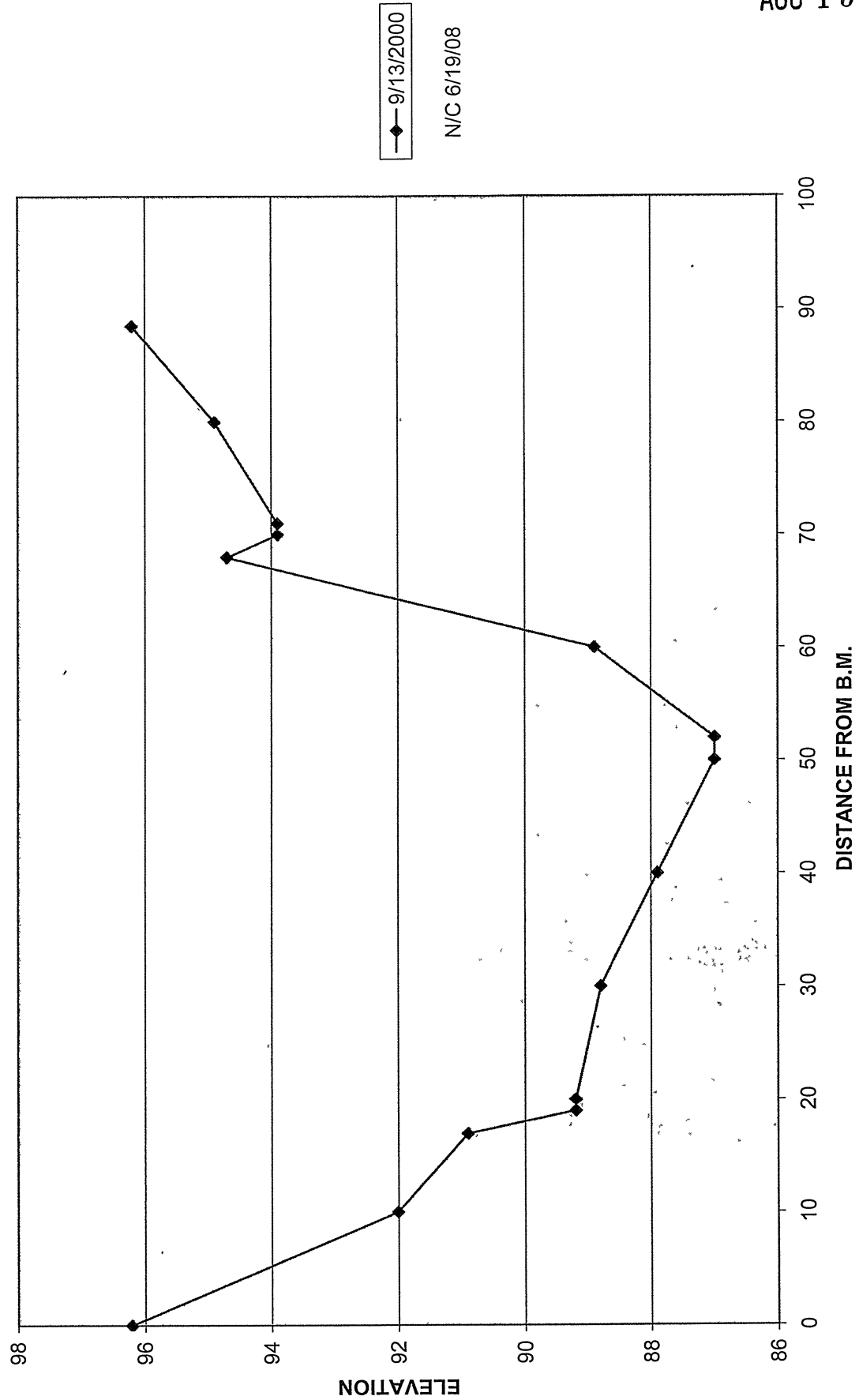
9/13/2000	
Distance from B.M.	Elevation
0	96.2
10	92
17	90.9
19	89.2
20	89.2
30	88.8
40	87.9
50	87
52	87
60	88.9
68	94.7
70	93.9
71	93.9
80	94.9
88.5	96.2

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09S82330001 UPSTREAM D.I.L.



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BRIDGE NO. -----

9-436-0.68

DATE :

9/13/2000

DOWNSTREAM GROUND ELEVATION @ EDGE OF BRIDGE

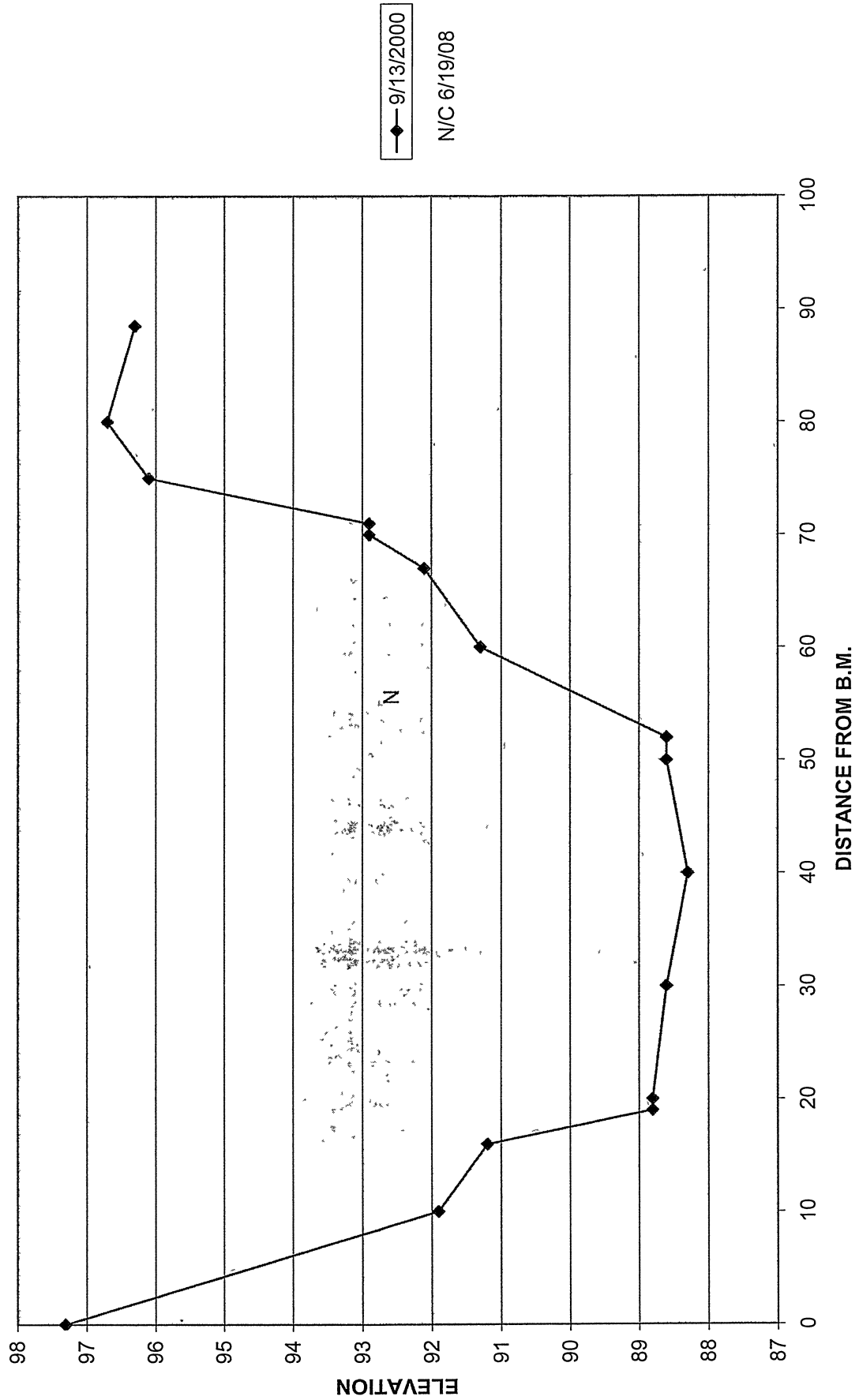
9/13/2000	
Distance from B.M.	Elevation
0	97.3
10	91.9
16	91.2
19	88.8
20	88.8
30	88.6
40	88.3
50	88.6
52	88.6
60	91.3
67	92.1
70	92.9
71	92.9
75	96.1
80	96.7
88.5	96.3

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09S82330001 DOWNSTREAM D.L.



AUG 15 2019

BRIDGE NO. -----

9-436-0.68

DATE :

9/13/2000

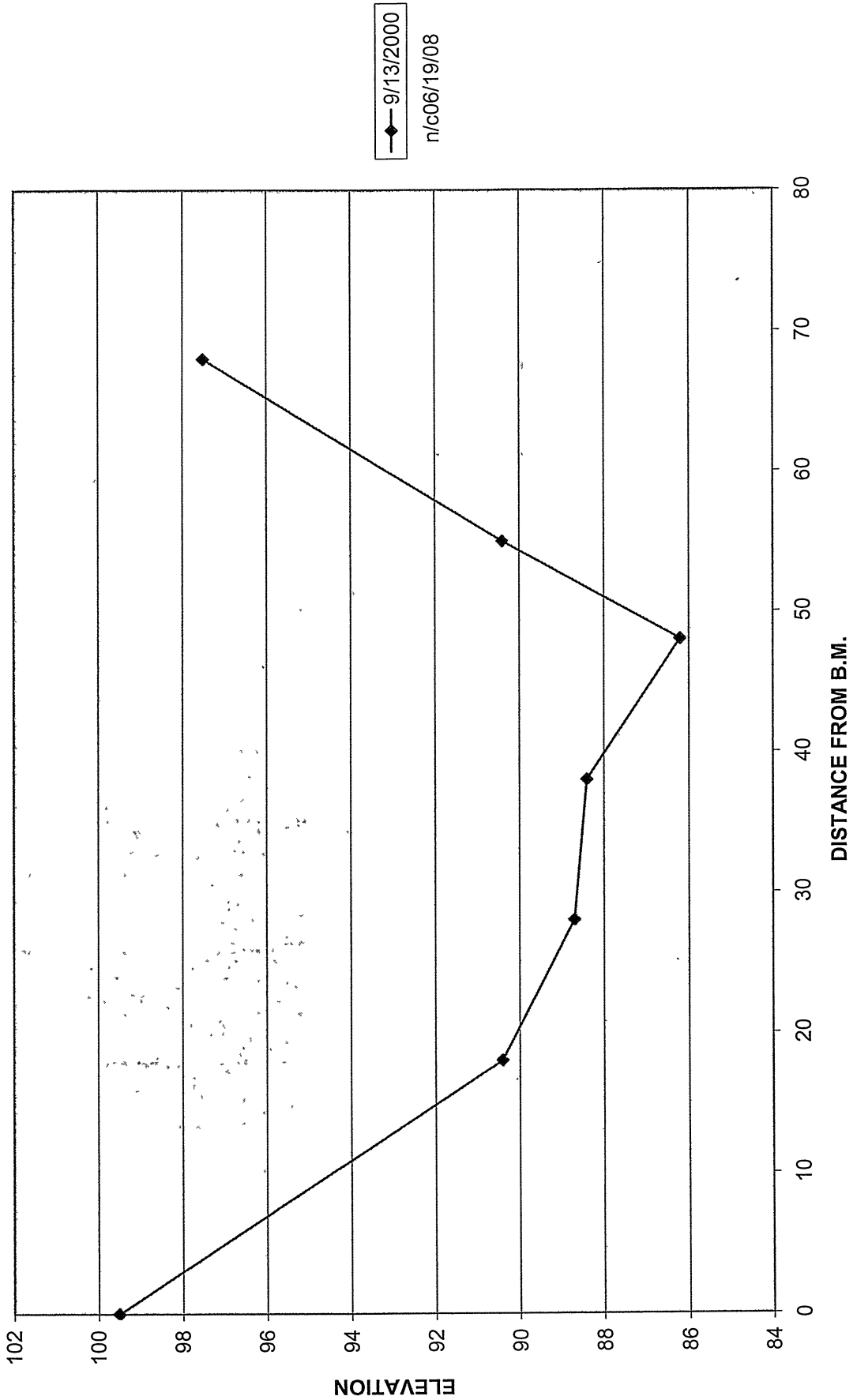
100' UPSTREAM, STREAMBED ELEVATIONS BANK TO BANK

9/13/2000	
Distance from B.M.	Elevation
0	99.5
18	90.4
28	88.7
38	88.4
48	86.2
55	90.4
68	97.5

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09S82330001 UPSTREAM 100'



AUG 15 2019

BRIDGE NO. -----

9-436-0.68

DATE :

9/13/2000

100' DOWNSTREAM, STREAMBED ELEVATIONS BANK TO BANK

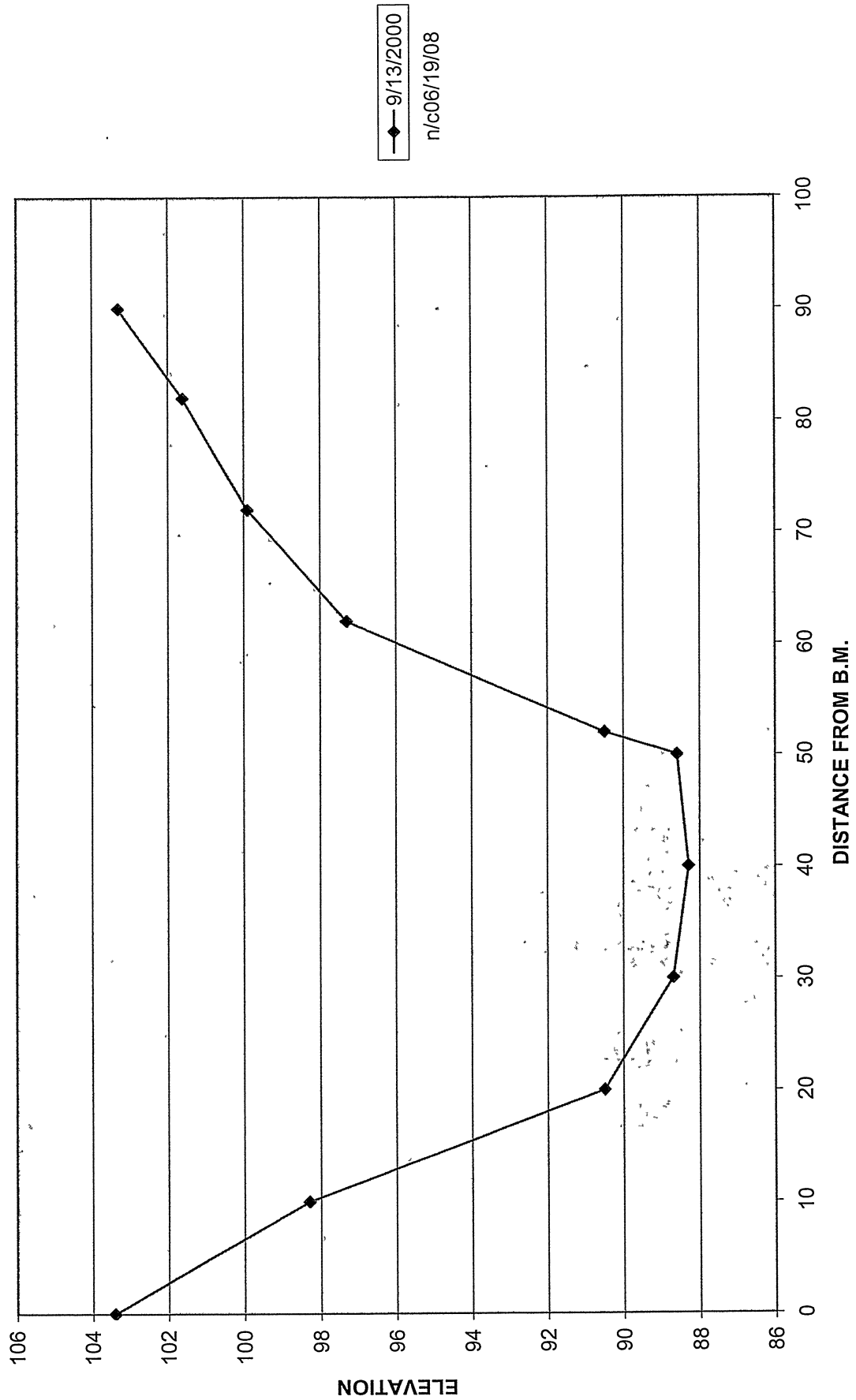
9/13/2000	
Distance from B.M.	Elevation
0	103.4
10	98.3
20	90.5
30	88.7
40	88.3
50	88.6
52	90.5
62	97.3
72	99.9
82	101.6
90	103.3

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09S82330001 DOWNSTREAM 100'



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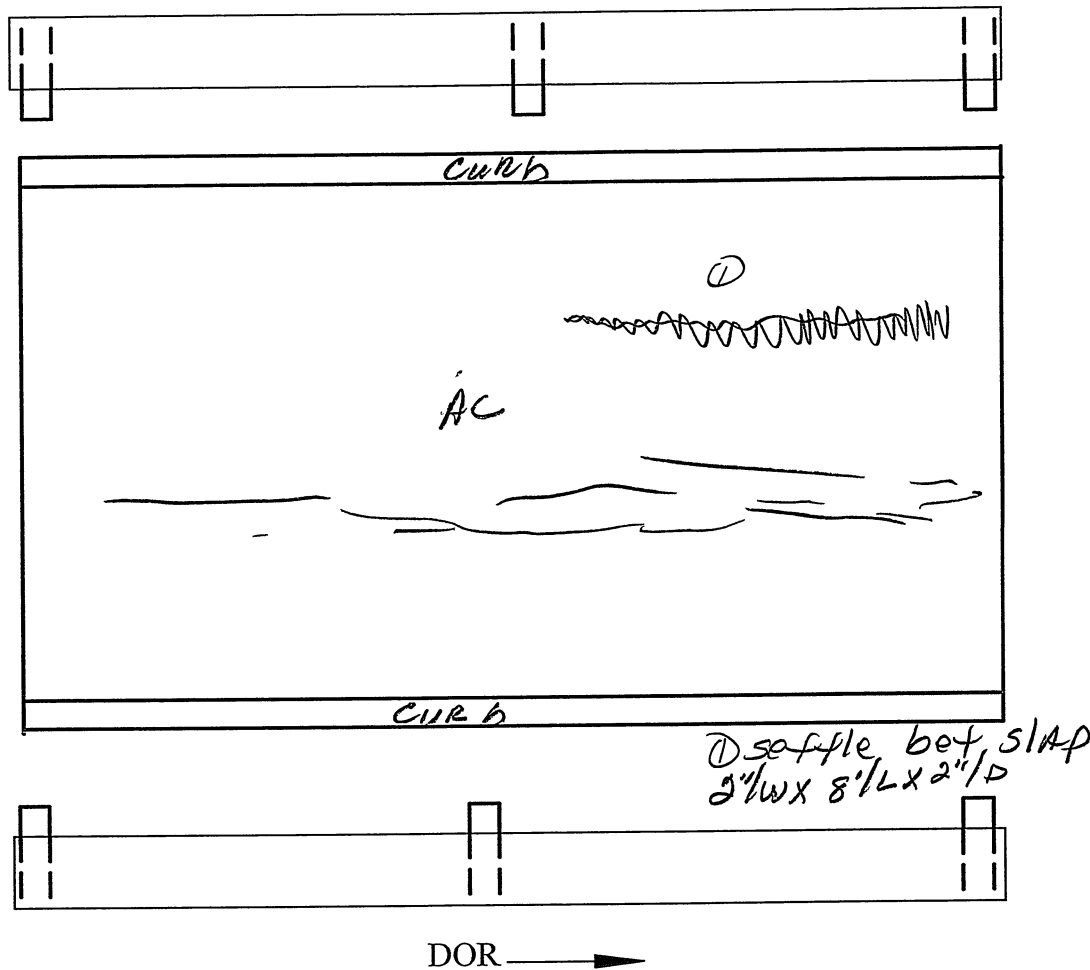
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Bridge No. 09 SR436 0.68

Skew 90 RT.

SPAN. NO. 1

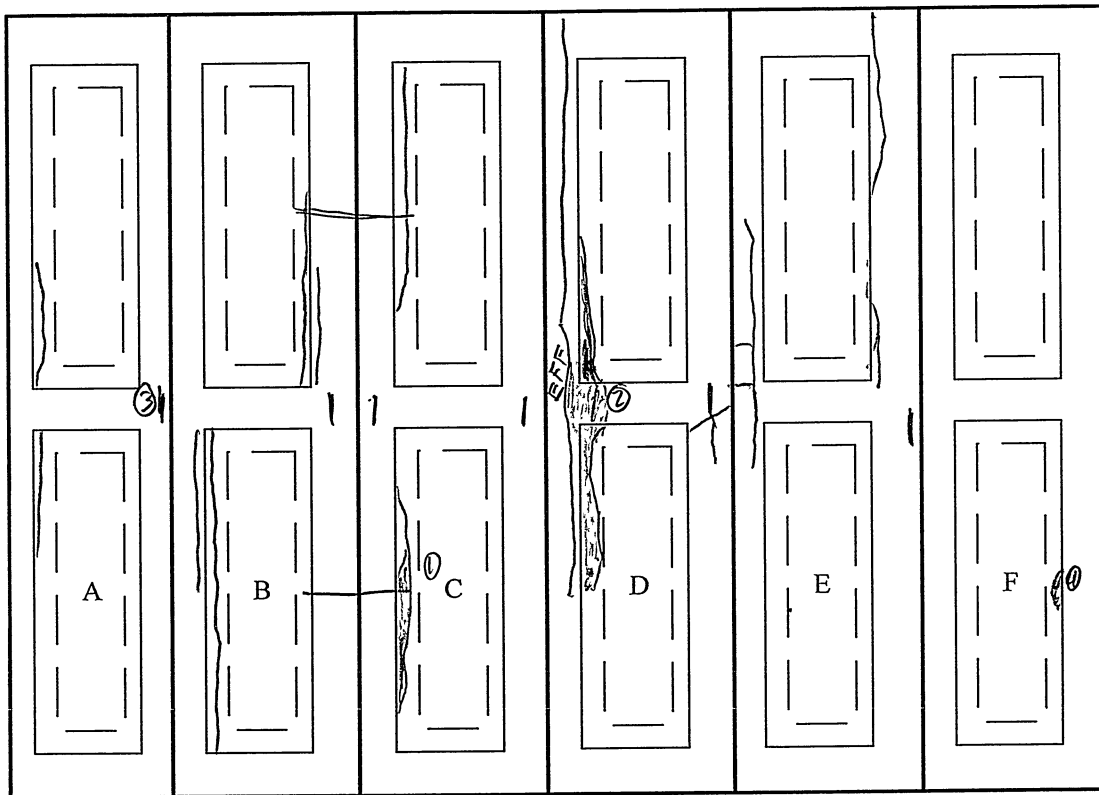
AUG 15 2019



ELEMENT	RATING	COMMENT
TOP DECK	G F P C	see ① & Fine cracks
CURBS	G F P C	
RAIL & POST	G F P C	
PAINT	G F P C	N/A
DRAINS	G F P C	
JOINT	G F P C	N/A
	G F P C	

09 SR436 0.68 90
 Bridge No. Co. Route Log Mile R/L Skew

AUG 15 2019
 SPAN. NO. /



ELEMENT		RATING	COMMENT
SLABS	A	G (F) P C	Hairline Cracks
	B	G (F) P C	Hairline - Fine cracks
	C	G (F) P C	① Beginning to spall 3' L X 3" W X 2" H Fine Cracks
	D	G (F) P C	Hairline to 1/8" cracks
	E	G (F) P C	② Beginning to spall 6' L X 4" W X 3" H up to 1/4" Cracks
	F	G (F) P C	④ spalling 6" L X 3" H X 1" D
BOLTS		G F P C	
		G F P C	
		G (F) P C	2 Missing
		G F P C	

Bridge No. 09 SR436 0.68

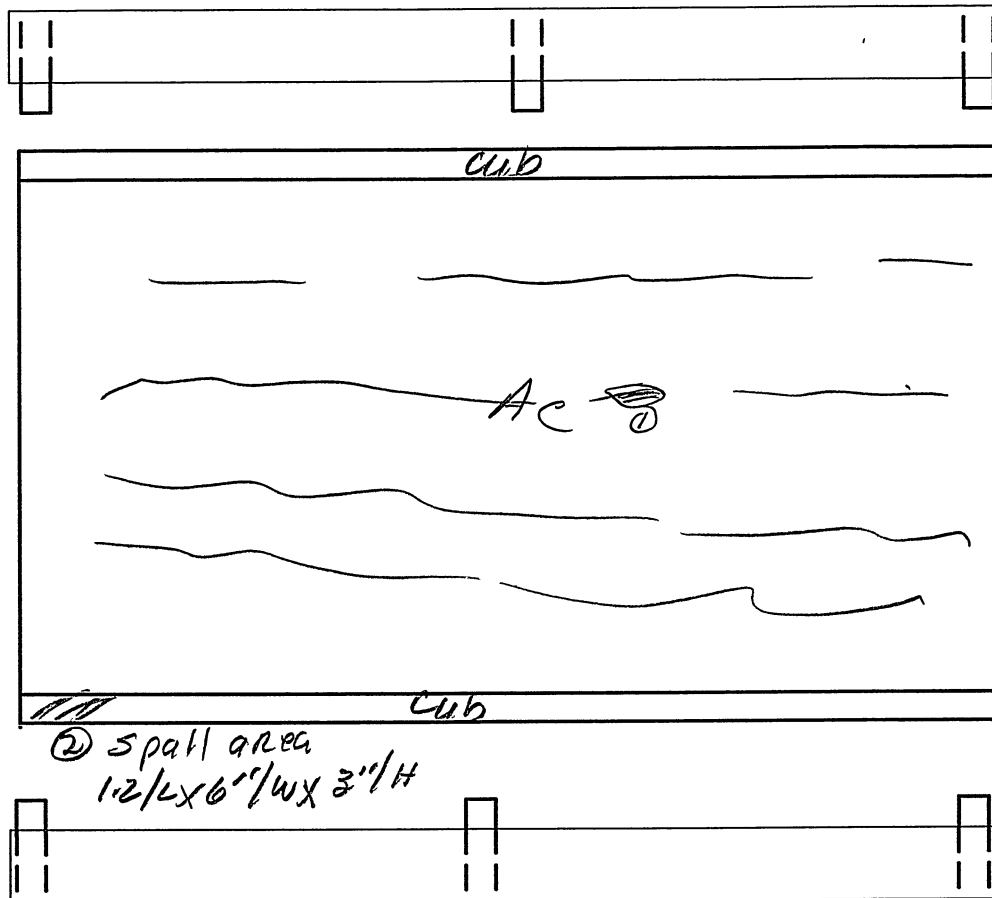
Skew 90 RT.

SPAN. NO. 2

AUG 15 2019

LS

① Spall area
6" L x 2" W x 1" D



② Spall area
12" L x 6" W x 3" H

DOR →

ELEMENT	RATING	COMMENT
TOP DECK	G <u>F</u> P C	Fine cracks & see ①
CURBS	G <u>F</u> P C	see ②
RAIL & POST	<u>G</u> F P C	
PAINT	G F P C	N/A
DRAINS	G F P C	
JOINT	G F P C	
	G F P C	

48 436 0.68
 Bridge No. Co. Route Log Mile R/L Skew

AUG 15 2019
 SPAN. NO. 2

A	B	C	D	E	F	G
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↑
 d.o.r.

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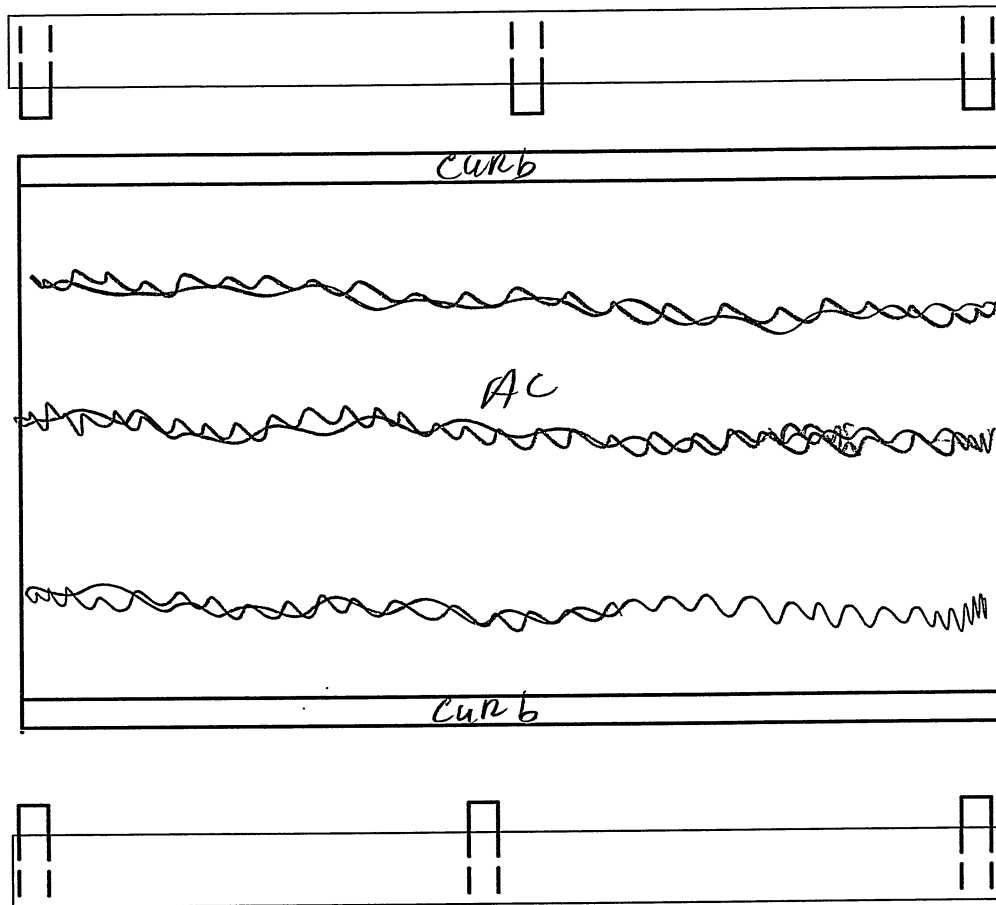
ELEMENT		RATING	COMMENT
BOX BEAM	A	G F P C	0 surface steel 6" L x 2" W
	B	G F P C	
	C	G F P C	
	D	G F P C	
	E	G F P C	
	F	G F P C	
	G	G F P C	
		G F P C	
		G F P C	
		G F P C	

Bridge No. 09 SR436 0.68

Skew 90 RT.

SPAN. NO. 3

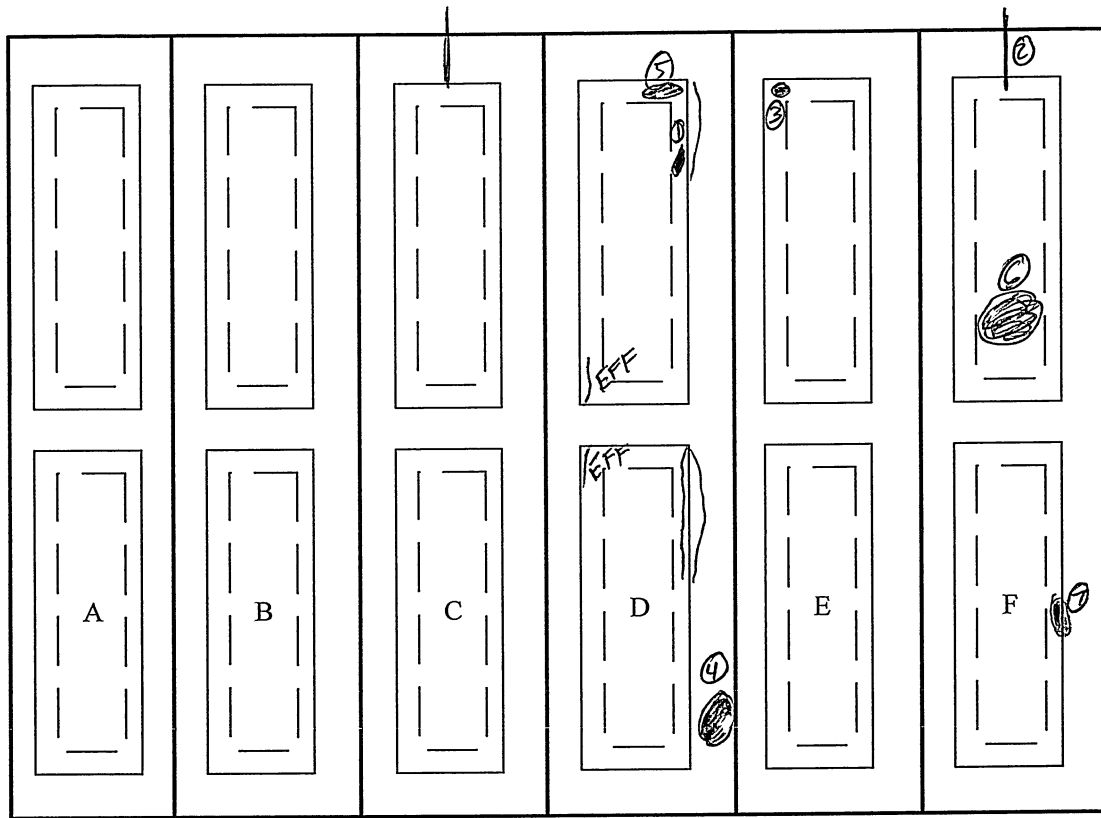
AUG 15 2019



ELEMENT	RATING	COMMENT
TOP DECK	G <u>F</u> P C	see ①
CURBS	<u>G</u> F P C	
RAIL & POST	<u>G</u> F P C	
PAINT	G F P C	N/A
DRAINS	<u>G</u> F P C	
JOINT	G F P C	N/A
	G F P C	

09 SR436 0.68 90
 Bridge No. Co. Route Log Mile R/L Skew

AUG 15 2019
 SPAN. NO. 3



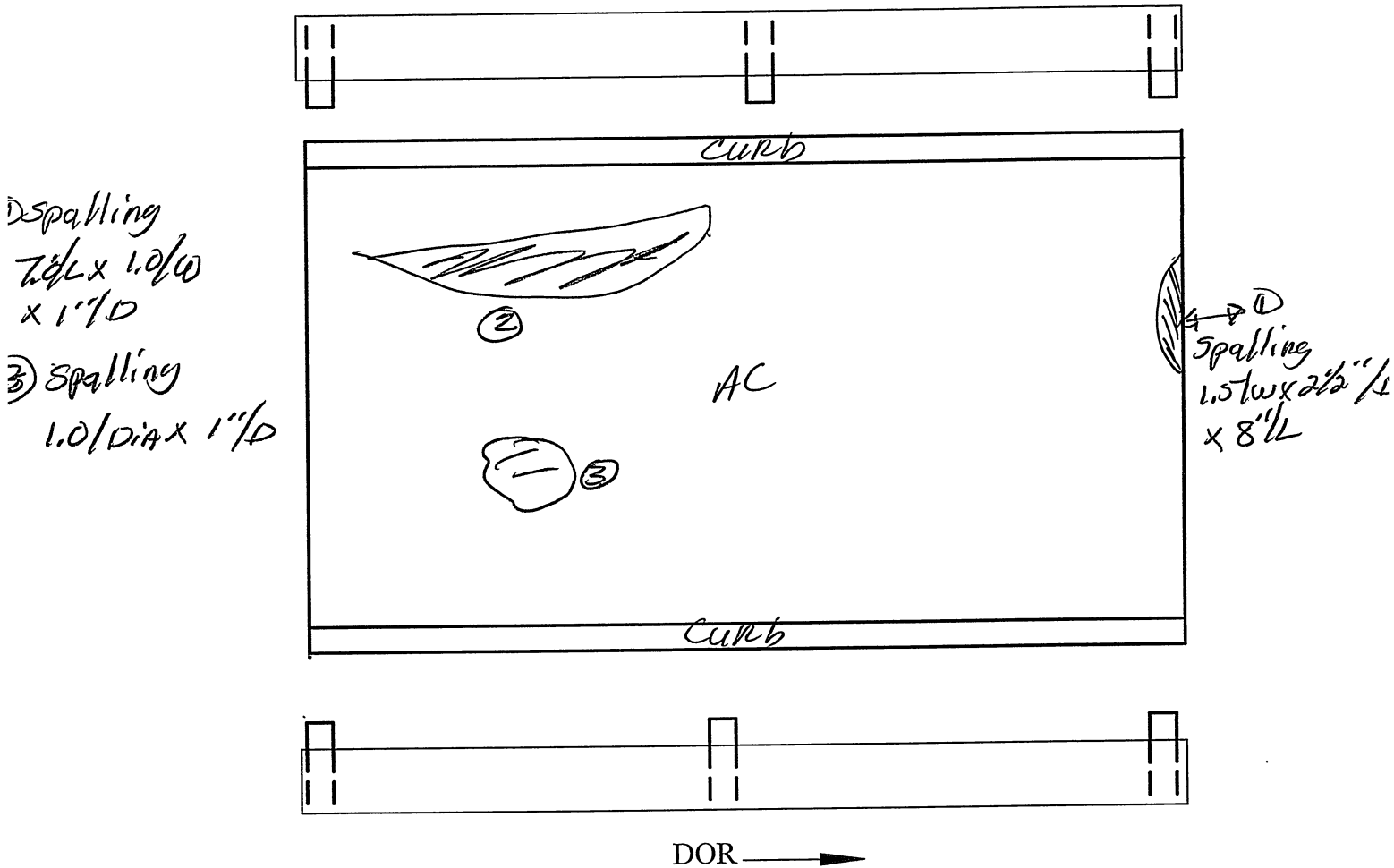
ELEMENT		RATING	COMMENT
SLABS	A	G F P C	
	B	G F P C	
	C	G F P C	
	D	G <u>F</u> P C	① SZS 5" dia x 1/2" D ④ Spalling 8" dia x 1" deep up to 1/8" cracks w/ EFF, ⑤ Beginning to spall 1'W x 4" H x 1/2" D
	E	G <u>F</u> P C	② SZS 3" dia x 1/2" D
	F	G <u>F</u> P C	③ Beginning to spall 2'W x 18" L ⑦ Spalling 6" L x 2" H x 1" D
BOLTS		G F P C	
		G <u>F</u> P C	1 missing
		G F P C	② Bolt Heavily Corroded

Bridge No. 09 SR436 0.68

Skew 90 RT.

SPAN 15 2019

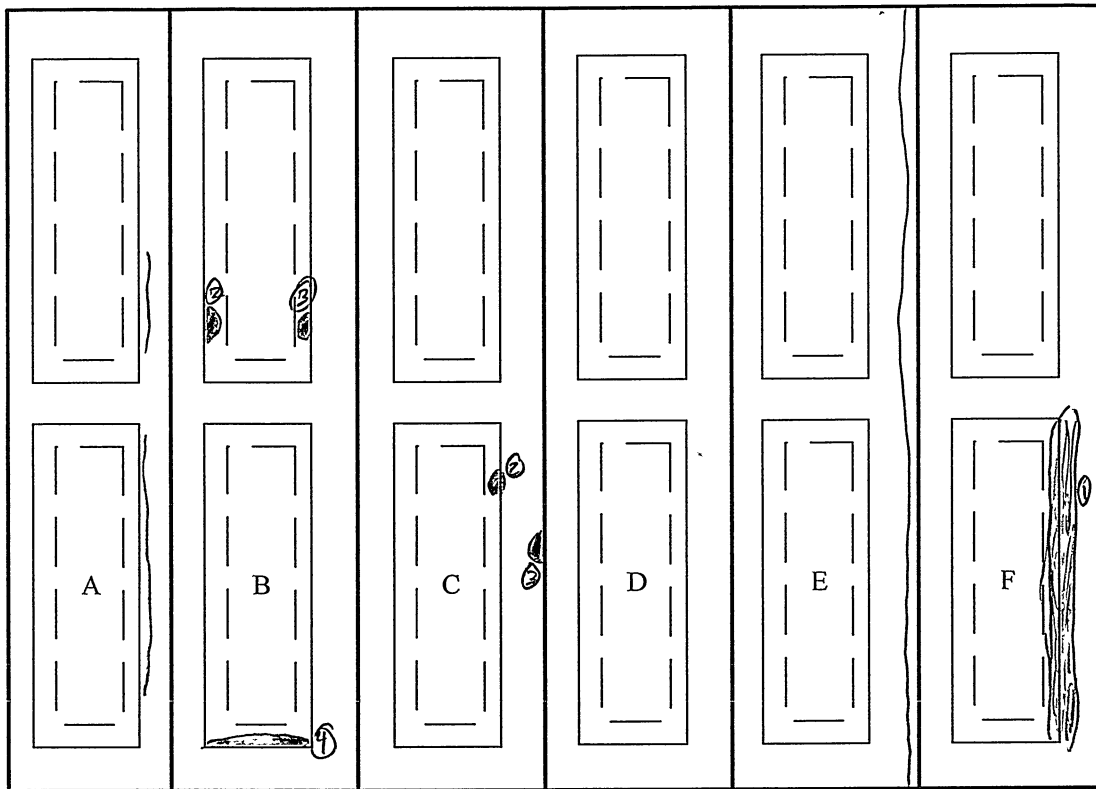
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ELEMENT	RATING	COMMENT
TOP DECK	G <u>Ⓟ</u> P C	see ①, ②, & ③
CURBS	<u>Ⓟ</u> F P C	
RAIL & POST	<u>Ⓟ</u> F P C	
PAINT	G F P C	N/A
DRAINS	<u>Ⓟ</u> F P C	
JOINT	G F P C	N/A
	G F P C	

09 SR436 0.68 90
 Bridge No. Co. Route Log Mile R/L Skew

AUG 15 2019
 SPAN. NO. 4



ELEMENT		RATING	COMMENT
SLABS	A	G F P C	Fine $\frac{1}{8}$ " cracks
	B	G F P C	① SZS 2'W X 3"D X 6"H see ② + ③
	C	G F P C	② spalling 6"H X 6"W X 1"D ③ spalling 4" dia X 1 1/2"D
	D	G F P C	
	E	G F P C	Hairline Crack w/ EFF
	F	G F P C	① Beginning to spall 8' L X 4"W X 8"H
BOLTS		G F P C	
		G F P C	
		G F P C	

AUG 15 2019

Rev. 08/03/00

Date: _____

BRIDGE NUMBER: 09S82330001 09 SR436 0068

Pg. # ____ of ____

CROSSING: REEDY CREEK

DATE 10/2/17

LAST EXPOSURE	ABUT/BENT/ PIER NUMBER	TOTAL HEIGHT TOP OF CAP TO (OR GROUND LINE/ DATE FOR PILES	(t) FOOTING THICKNESS	W/FTG @ H= TOP OF CAP TO TOP OF FOOTING	EXPOSURE
6.1'	A-1				6.0
8.6'	B-1A				9.5
9.8'	B-1B				10.0
11.1'	B-2A				11.5
10.5'	B-2B				11.5
7.8'	B-3				7.7
5.8'	A-2				6.0

TOP OF CAP TO TOP OF WATER: _____ RIP-RAP: YES: (X) NO: ()

100.00' UPSTREAM: _____

@ ABUTMENTS: 1-2

THRU STRUCTURE: _____

@ BENTS/PIERS: 1

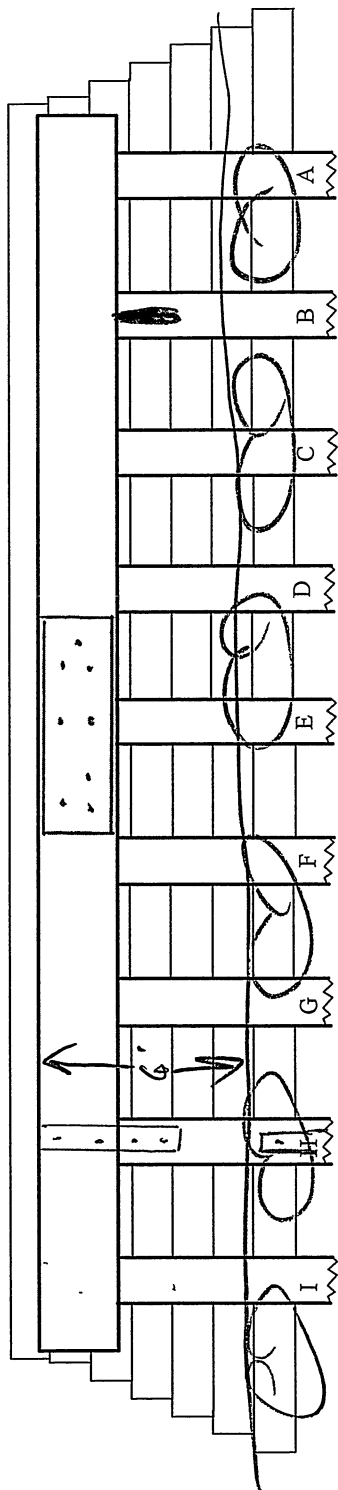
100.00' DOWNSTREAM: _____

UPSTREAM ☐DOWNSTREAM ☐THRU STRUCTURE ☒

COMMENTS: _____

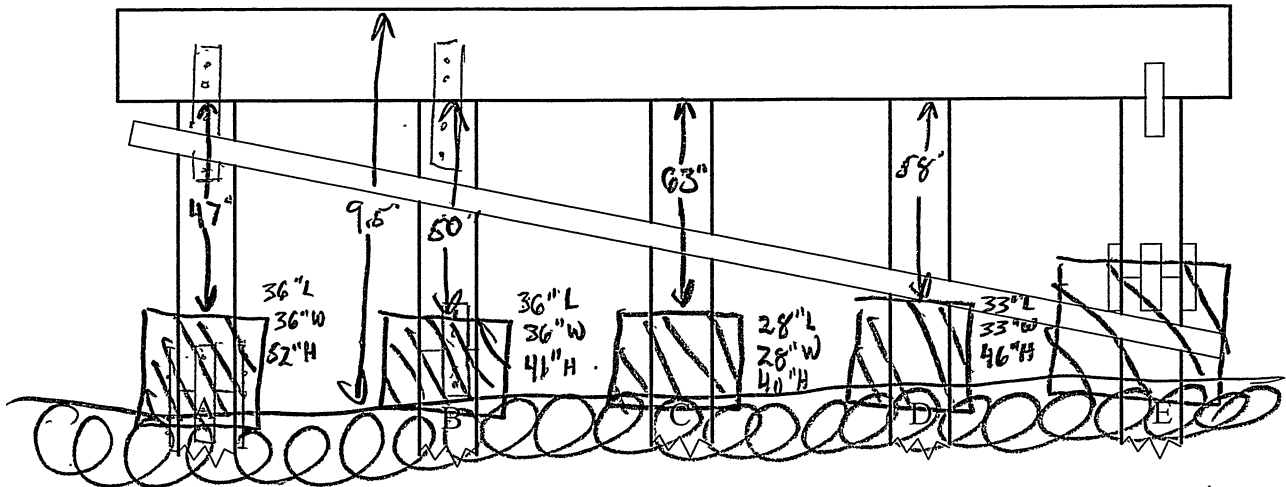
Rev. 08/03/00

Date: _____

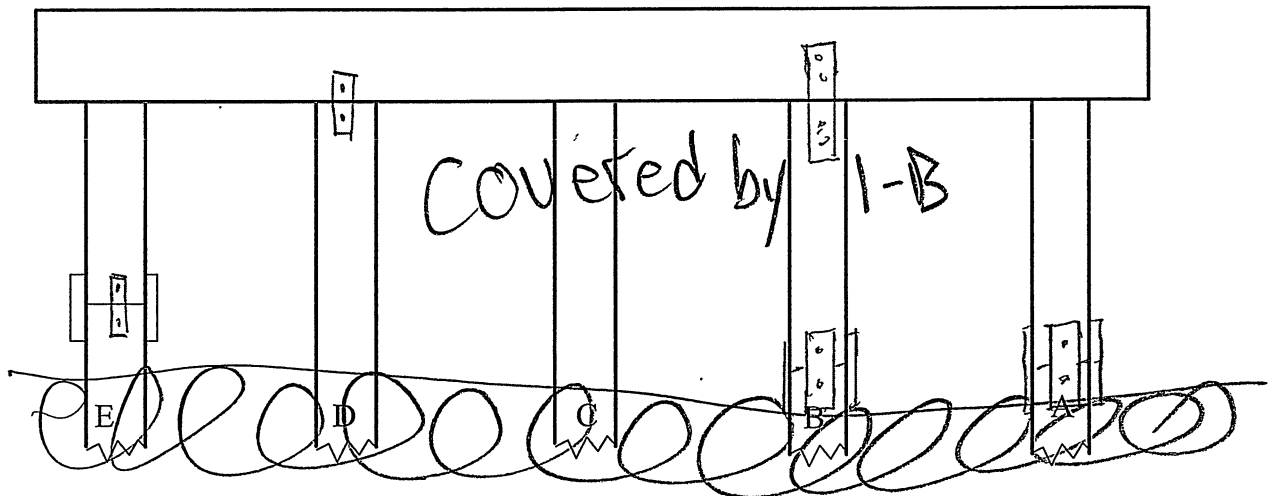


LOOKING BACK

ELEMENT	RATING	COMMENT
CAP	G F P C	med. weathering
WINGS	G F P C	med. weathering
PILES A - I	G F P C	replaced by 'A' and 'C'
B	G F P C	
	G F P C	
	G F P C	
	G F P C	
	G F P C	med. weathering
BREASTWALL	G F P C	
EMB.	G F P C	
VEG.	G F P C	Heavy scattered
RIP - RAP	G F P C	



FRONT VIEW



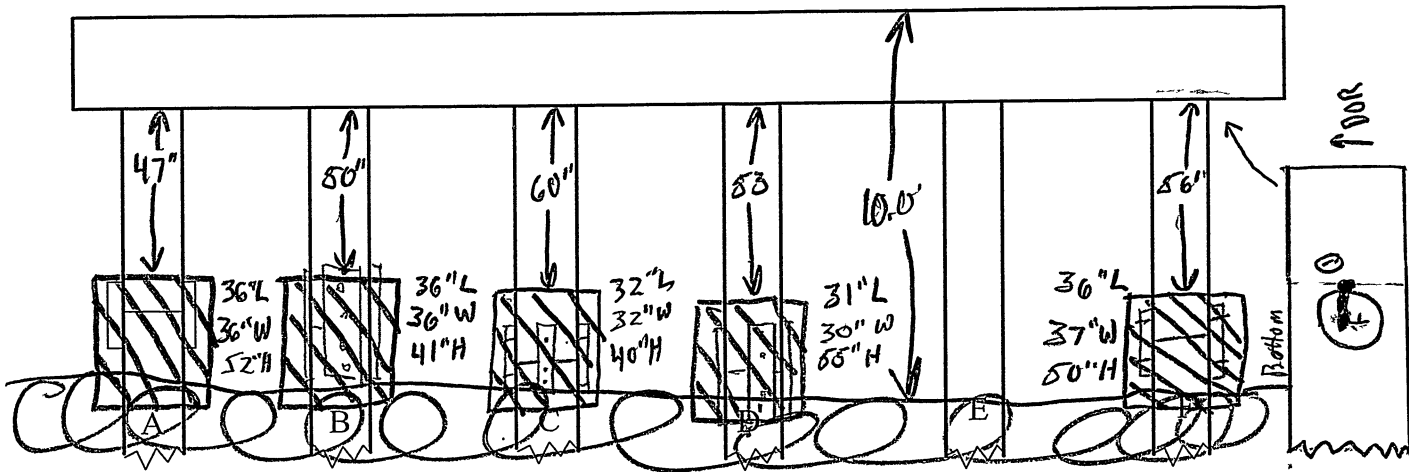
REAR VIEW

ELEMENT	RATING	COMMENT
CAP	G <u>F</u> P C	med. weathering
PILES A	G <u>F</u> P C	
B	G <u>F</u> P C	
C	G <u>F</u> P C	
D	G <u>F</u> P C	
E	G <u>F</u> P C	
RIP - RAP	G <u>F</u> P C	light weathering
BRACING	G <u>F</u> P C	
Lossing	G <u>F</u> P C	

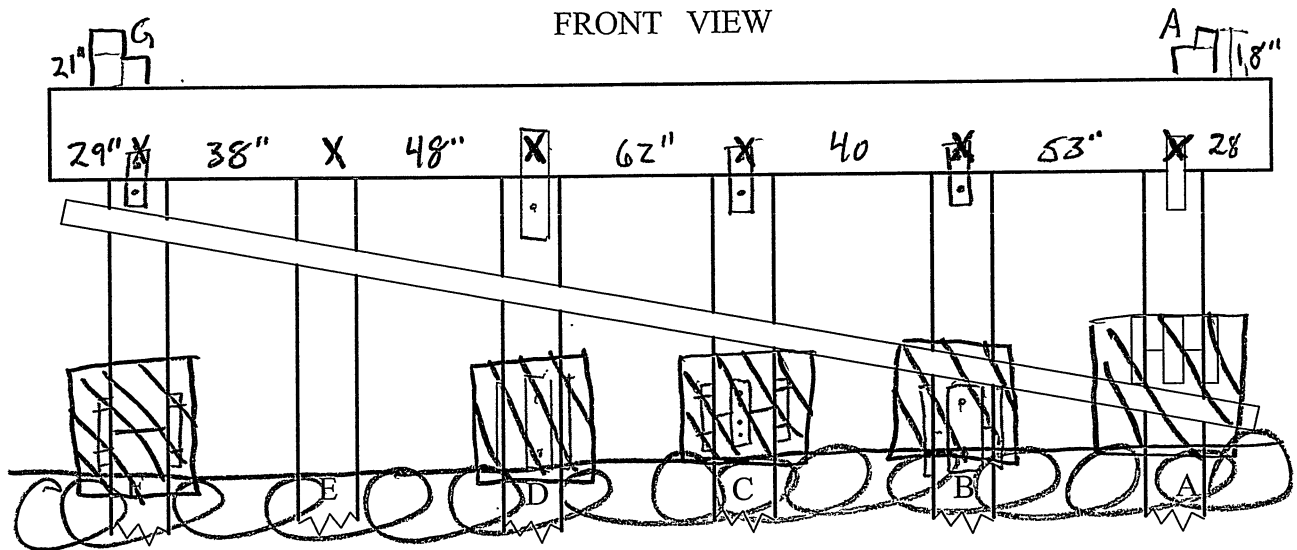
AUG 15 2019

09 -- SR436 -- 0.68
Bridge No. Co. Route Log Mile

BENT. NO. 1 - B



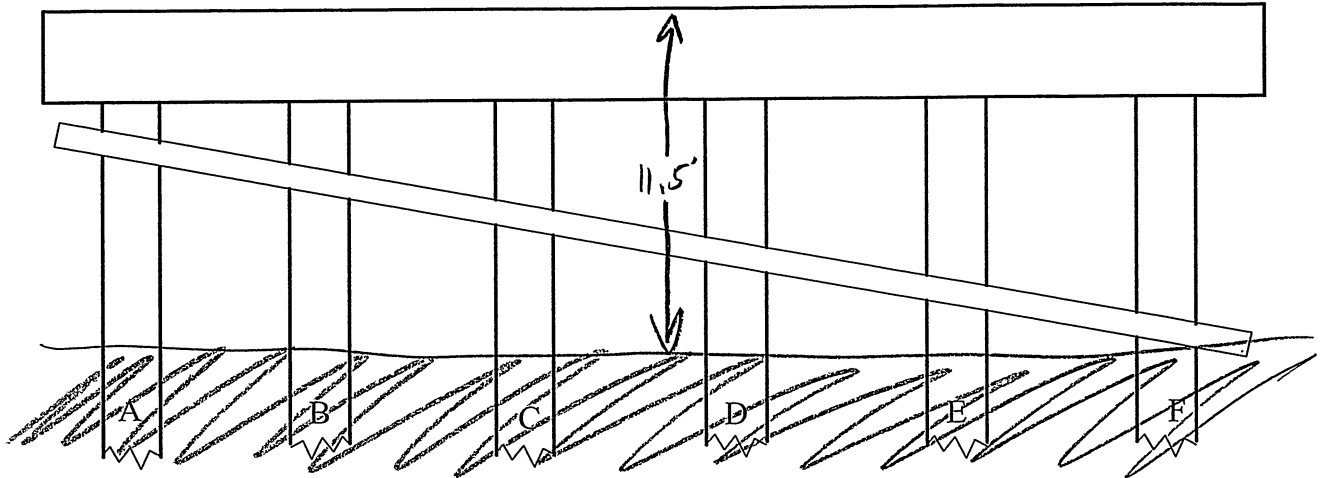
FRONT VIEW



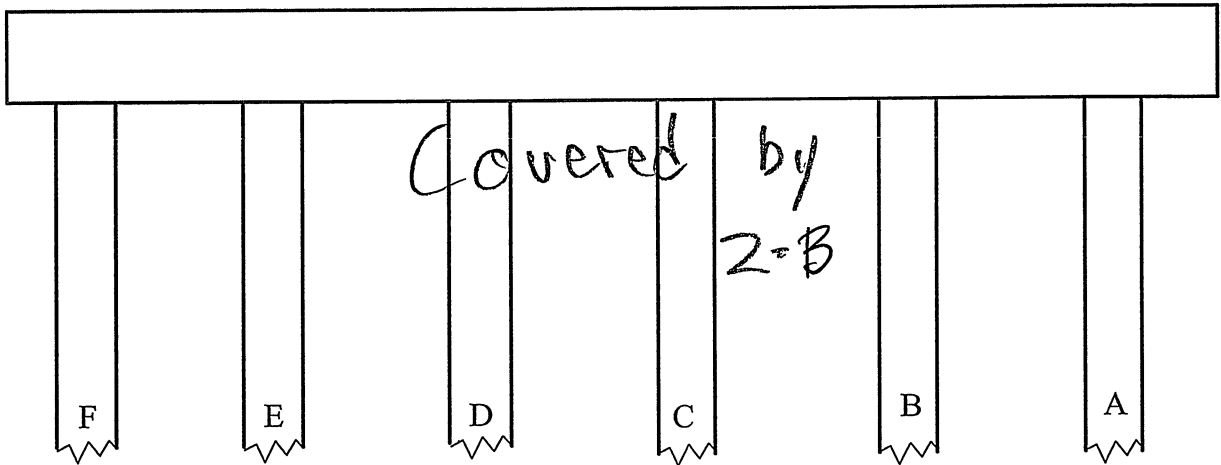
REAR VIEW

ELEMENT	RATING	COMMENT
CAP 12"x14" L	G F <u>P</u> C	① Decay 8" Wx 2' Lx 3" D
PILES A 11"	G <u>F</u> P C	med. weathering
B 11"	G <u>F</u> P C	
C 12	G <u>F</u> P C	
D 10"	G <u>F</u> P C	
E 12"	G <u>F</u> P C	
F 10"	G <u>F</u> P C	
RIP - RAP	<u>G</u> F P C	
BRACING	G <u>F</u> P C	light weathering
Conc casing	<u>G</u>	

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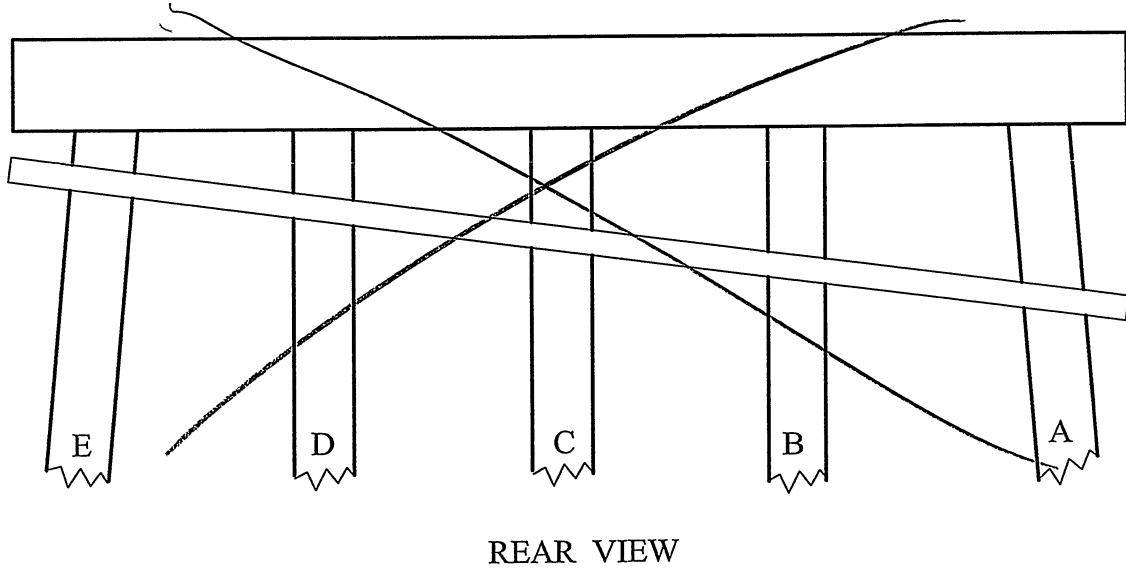
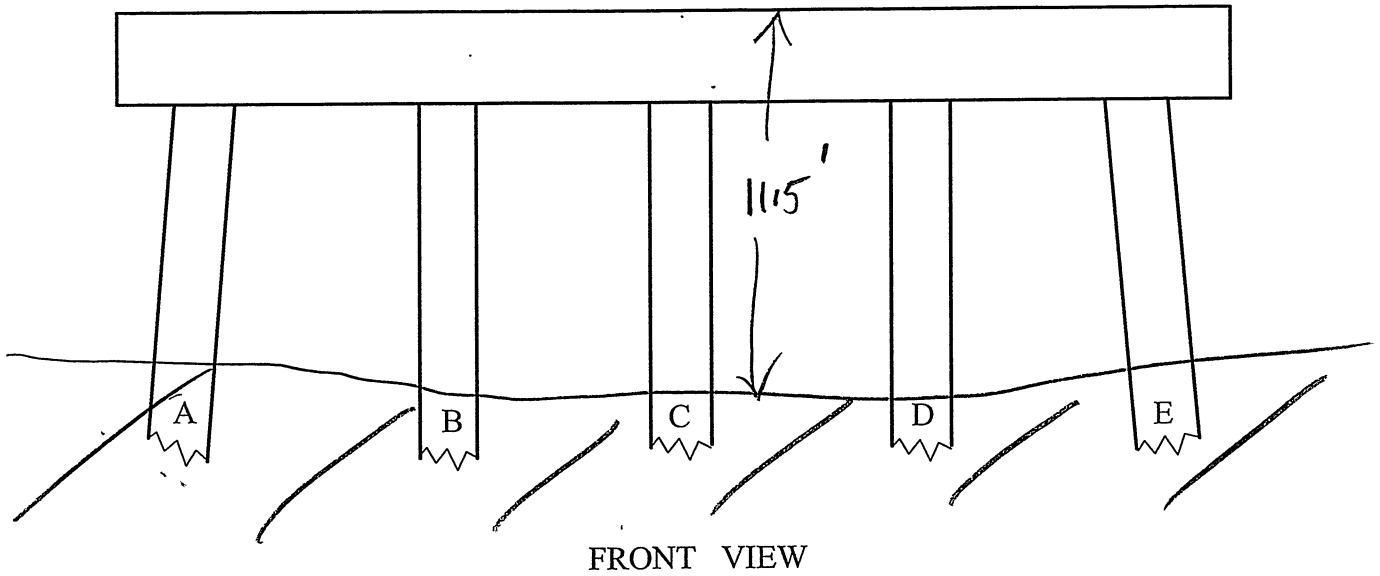


FRONT VIEW



REAR VIEW

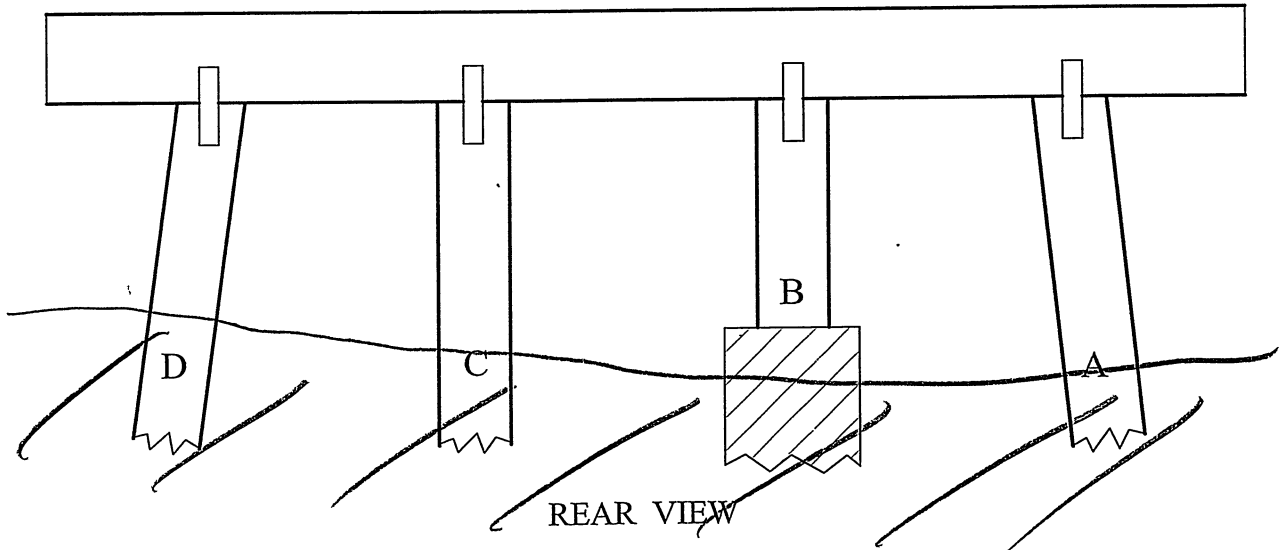
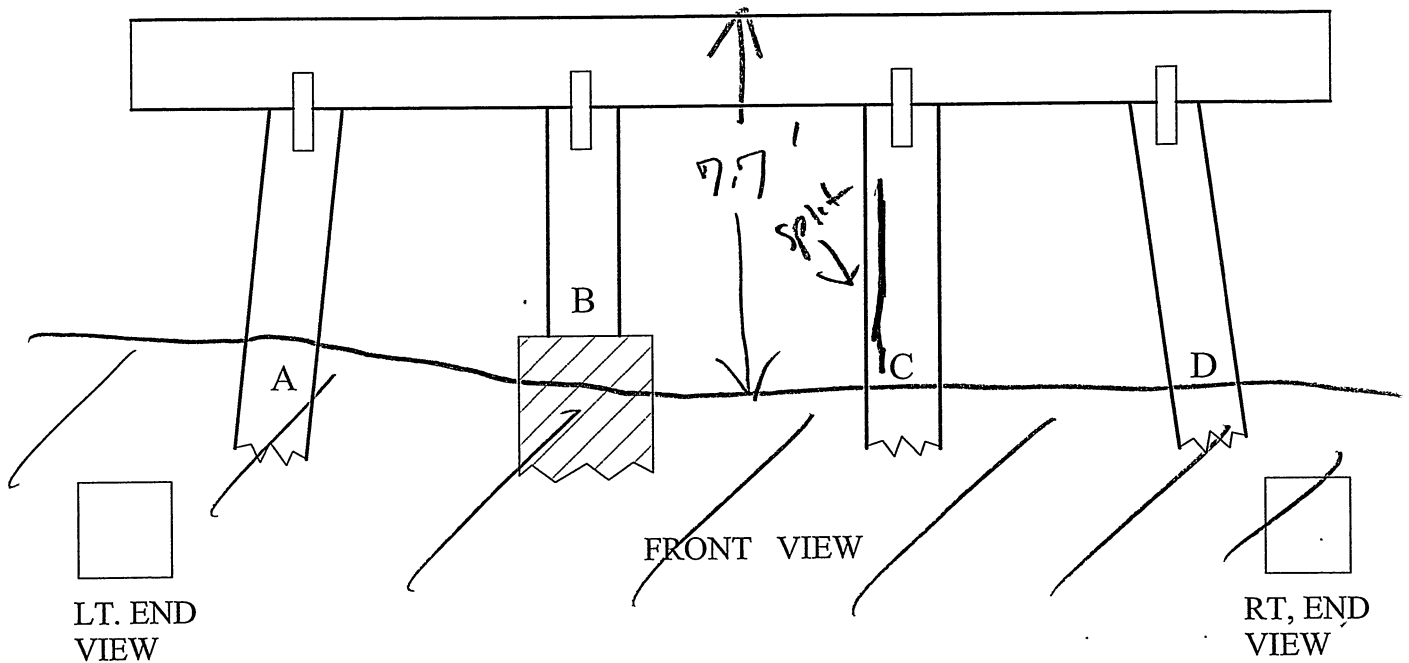
ELEMENT	RATING	COMMENT
CAP	G <u>F</u> P C	light to med weathering
PILES A	G <u>F</u> P C	
B	G <u>F</u> P C	
C	G <u>F</u> P C	
D	G <u>F</u> P C	
E	G <u>F</u> P C	
F	G <u>F</u> P C	
RIP - RAP	G F P C	N/A
BRACING	G <u>F</u> P C	



ELEMENT	RATING	COMMENT
CAP	G F P C	NIV covered by 2/A medium weathering ↓
PILES A	G F P C	
B	G F P C	
C	G F P C	
D	G F P C	
E	G F P C	
RIP - RAP	G F P C	N/A
BRACING	G F P C	↓

Bridge No. Co. Route Log Mile

BENT. NO. 3

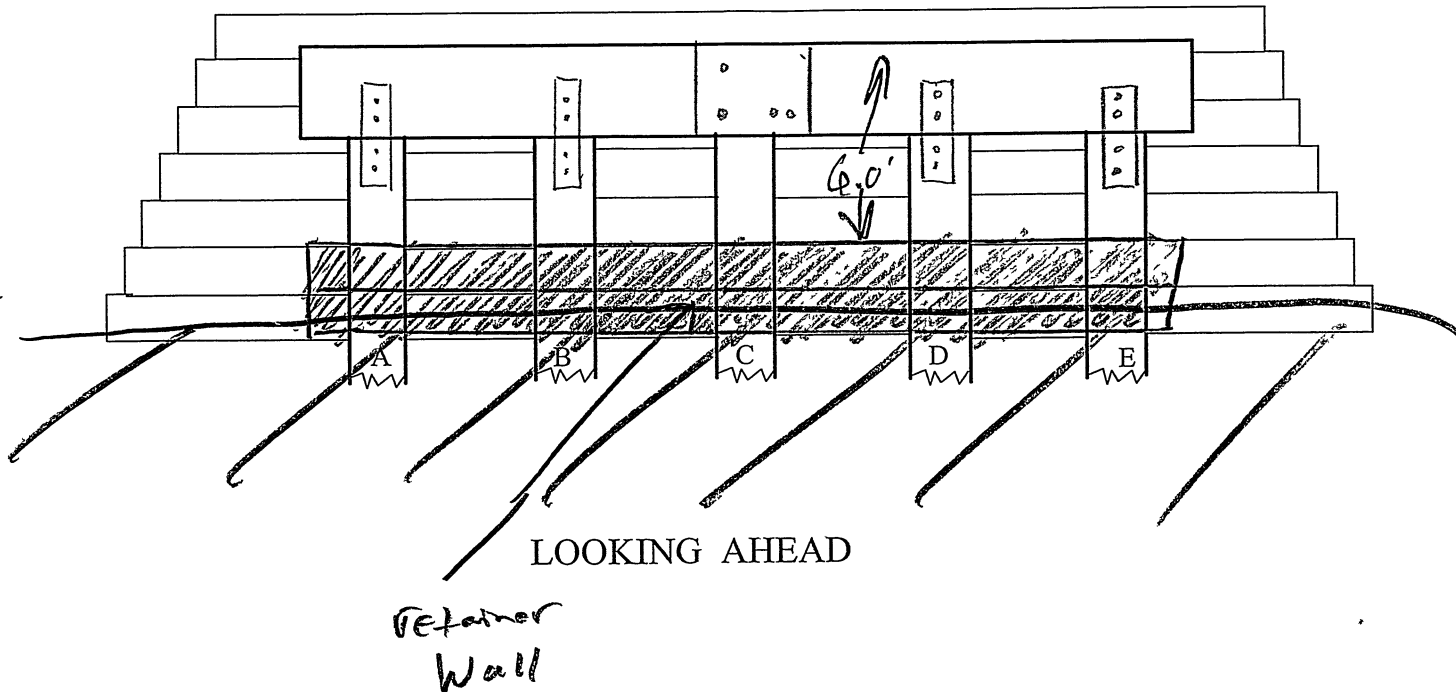


ELEMENT	RATING	COMMENT
CAP	G <u>F</u> P C	Light decay/medium weathering ↓ 1" split 4' H x 2" depth / 13" pile ↓
PILES A	G <u>F</u> P C	
B	G <u>F</u> P C	
C	G <u>F</u> P C	
D	G <u>F</u> P C	
CONC. CASING	G <u>F</u> P C	
	G F P C	

AUG 15 2019

09 -- SR436 -- 0.68

Bridge No. Co. Route Log Mile

ABUT. NO. 2

ELEMENT	RATING	COMMENT
CAP	G <u>F</u> P C	Light to medium weathering
WINGS	G <u>F</u> P C	
PILES A	G <u>F</u> P C	
B	G <u>F</u> P C	
C	G <u>F</u> P C	
D	G <u>F</u> P C	
E	G <u>F</u> P C	Heavy growth
BREASTWALL	G <u>F</u> P C	
EMB.	<u>G</u> <u>F</u> P C	
VEG.	G F <u>P</u> C	
RIP-RAP	G <u>F</u> P C	Scattered and galled/ Light weathering
Retainer Wall	G <u>F</u> P C	
	G F P C	

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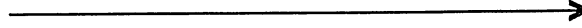
<u>09S82330001</u>	<u>09</u>	<u>SR436</u>	<u>0068</u>	
BRIDGE NO.:	CO.	ROUTE	L.M.	L/R

SKEW: 90

No. of Spans: 4

No. of Approach Spans:

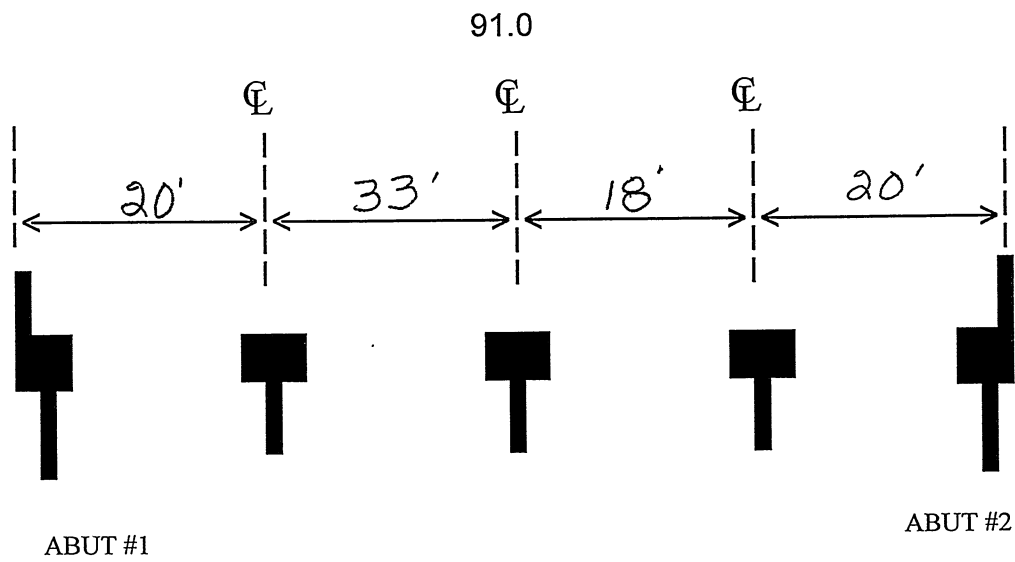
Direction of Route



PLAN VIEW

REQUIRED DATA:

1. F = FIXED
E = EXPANSION
2. S = SIMPLE
C = CONTINUOUS SUPPORT



Your Agency Name

Your Office Name

Your Department Name

Structure Inventory and Appraisal Sheet (English Units)

Bridge Key: 09S82330001 Agency ID: 09S82330001 SR: 45.0 SD/FO: SD

IDENTIFICATION

State 1: 47 Tennessee Struc Num 8 09S82330001
 Facility Carried 7: FAS 436 Location 9 27 MI NE JCT SR77&SR105
 Rte (On/Under) 5A: Route On Structure Rte Signing Prefix 5B: 4 County Hwy
 Level of Service 5C: 1 Mainline Route Number 5D: 00436
 Directional Suffix 5E: 0 N/A (NBI) % Responsibility Unknown
 SHD District 2: Region 4 County Code 3 Carroll
 Place Code 4: Unknown Mile Post 11: 0 680 mi
 Feature Intersected 6: REEDY CREEK
 Latitude 16 36° 00' 52" Longitude 17 088° 32' 26"
 Border Bndg Code 98 Unknown (P)
 Border Bndg Number 99 Unknown

INSPECTION

Frequency 91: 24 months Inspection Date 90 8/15/2019 Next Inspection 8/15/2021
 FC Frequency 92A: NA FC Inspection Date 93A: NA Next FC Inspection NA
 UW Frequency 92B: NA UW Inspection Date 93B: NA Next UW Inspection NA
 SI Frequency 92C: NA SI Date 93C: NA Next SI: NA
 Element Frequency: 24 months Element Insp. Date: 8/15/2019 Next Elem Insp: 8/15/2021

CLASSIFICATION

Defense Highway 100: 0 Not a STRAHNET hwy Parallel Structure 101: No || bndg exists
 Direction of Traffic 102: 2 2-way traffic Temporary Structure 103: Unknown (NBI)
 Highway System 104: 0 Not on NHS NBIS Length 112 Long Enough
 Toll Facility 20 3 On free road Functional Class 26 07 Rural Mjr Collector
 Defense Hwy 110 0 Not a STRAHNET hwy Historical Significance 37 4 Hist sign not determin
 Owner 22: 01 State Highway Agency
 Custodian 21: 01 State Highway Agency

STRUCTURE TYPE AND MATERIALS

Number of Approach Spans 46 3 Number of Spans Main Unit 45 1
 Main Span Material Design 43 A/B
 5 Prestressed Concrete
 Approach Span Material Design 44 A/B.
 1 Concrete
 Deck Type 107: 9 Other
 Wearing Surface 108A: 1 Monolithic Concrete
 Membrane 108B 0 None
 Deck protection 108C None

CONDITION

Deck 58: 5 Fair Super 59: 5 Fair Sub 60: 4 Poor
 Culvert 62: N N/A (NBI) Channel/Channel Protection 61: 6 Bank Slumping

AGE AND SERVICE

Year Built 27: 1960 Year Reconstructed 106 -1
 Type of Service on 42A 1 Highway
 Type of Service under 42B 5 Waterway
 Lanes on 28A: 2 Lanes under 28B 0 Detour Length 19: 6 2 mi
 ADT 29 280 Truck ADT 109 7% Year of ADT 30 2013

LOAD RATING AND POSTING

Inventory Rating Method 65 B ASD (metric tons) Operating Rating Method 63: B ASD (metric tons)
 Inventory Rating 66: HS16 4 Operating Rating 64 HS24 3
 Design Load 31: 2 M 13 5 (H 15) Posting 70 5 At/Above Legal Loads
 Posting Status 41: A Open, no restriction

APPRAISAL

Bridge Rail 36A: 0 Substandard Approach Rail 36C: 0 Substandard
 Transition 36B: 0 Substandard Approach Rail Ends 36D 0 Substandard
 Str Evaluation 67: 4 Minimum Tolerable Deck Geometry 68 3 Intolerable - Correct
 Underclearance, Vertical and Horizontal 69: N Not applicable (NBI)
 Waterway Adequacy 71: 6 Equal Minimum Approach Alignment 72: 8 Equal Desirable Cnt
 Scour Critical 113: 3 SC - Unstable

PROPOSED IMPROVEMENTS

Bridge Cost 94: \$695,000 Type of Work 75 31 Repl-Load Capacity
 Roadway Cost 95 \$70,000 Length of Improvement 76 120 1 ft
 Total Cost 96 \$1,043,000 Future ADT 114: 448
 Year of Cost Estimate 9 2014 Year of Future ADT 115 2034

NAVIGATION DATA

Navigation Control 38 Permit Not Required
 Vertical Clearance 39 0 0 ft Horizontal Clearance 40 0 0 ft
 Pier Protection 111: Unknown (NBI) Lift Bridge Vertical Clearance 116

GEOMETRIC DATA

Length Max Span 48 33 14 ft Structure Length 49 89 90 ft
 Curb/Sdwk Width L 50A 0 00 ft Curb/Sidewalk Width R 50B 0 00 ft
 Width Curb to Curb 51: 20 34 ft Width Out to Out 52: 21 65 ft
 Approach Roadway width 32: (w/ shoulders) 27 69 ft Median 33 0 No median
 Deck Area: 1,948 27 sq ft
 Skew 34: 0 00° Structure Flared 35 0 No flare
 Vertical Clearance 10 99 99 ft Horizontal Clearance 47: 20 34 ft
 Minimum Vertical Clearance Over Bridge 53 99 99 ft
 Minimum Vertical Underclearance Reference 54A N Feature not hwy or RR
 Minimum Vertical Underclearance 54B 0 00 ft
 Minimum Lateral Underclearance Reference R 55A N Feature not hwy or RR
 Minimum Lateral Underclearance R 55 0 00 ft
 Minimum Lateral Underclearance L 56 0 00 ft

Your Agency Name

Your Office Name

Your Department Name

Structure Inventory and Appraisal Sheet (English Units)

ELEMENT CONDITION STATE DATA

Str Unit	Elm/Env	Description	Unit	Total Qty	% in 1	Qty. St. 1	% in 2	Qty. St. 2	% in 3	Qty. St. 3	% in 4	Qty. St. 4
0	12/3	Re Concrete Deck	sq.ft	2,002.00	100%	2,002.00	0%	0.00	0%	0.00	0%	0.00
0	510/3	Wearng Surfaces	sq.ft	1,911.00	96%	1,841.00	4%	70.00	0%	0.00	0%	0.00
0	3210/3	Del/Spall/Patch/Pot(Wear Surf)	sq.ft	20.00	0%	0.00	100%	20.00	0%	0.00	0%	0.00
0	3220/3	Crack (Wearng Surface)	sq.ft	50.00	0%	0.00	100%	50.00	0%	0.00	0%	0.00
0	109/3	Pre Opn Conc Girder/Beam	ft	231.00	100%	231.00	0%	0.00	0%	0.00	0%	0.00
0	116/3	Re Conc Stringer	ft	348.00	81%	283.00	16%	55.00	3%	10.00	0%	0.00
0	1080/3	Delamination/Spall/Patched Area	ft	25.00	0%	0.00	100%	25.00	0%	0.00	0%	0.00
0	1090/3	Exposed Rebar	ft	2.00	0%	0.00	100%	2.00	0%	0.00	0%	0.00
0	1120/3	Efflorescence/Rust Staining	ft	8.00	0%	0.00	100%	8.00	0%	0.00	0%	0.00
0	1130/3	Cracking (RC and Other)	ft	30.00	0%	0.00	67%	20.00	33%	10.00	0%	0.00
0	216/3	Timber Abutment	ft	44.00	100%	44.00	0%	0.00	0%	0.00	0%	0.00
0	228/3	Timber Pile	(EA)	26.00	96%	25.00	4%	1.00	0%	0.00	0%	0.00
0	1170/3	Split/Delamination (Timber)	(EA)	1.00	0%	0.00	100%	1.00	0%	0.00	0%	0.00
0	235/3	Timber Pier Cap	ft	105.00	99%	104.00	0%	0.00	1%	1.00	0%	0.00
0	1140/3	Decay/Section Loss	ft	1.00	0%	0.00	0%	0.00	100%	1.00	0%	0.00
0	331/3	Re Conc Bridge Railing	ft	182.00	100%	182.00	0%	0.00	0%	0.00	0%	0.00